

126-A

SIXTH BIENNIAL REPORT

OF THE

STATE ROAD  
DEPARTMENT

OF THE

STATE OF FLORIDA



FOR THE PERIOD

BEGINNING JANUARY 1, 1925

AND

ENDING DECEMBER 31, 1926

F625.75  
R7

FLORIDA STATE LIBRARY



# SIXTH BIENNIAL REPORT

OF THE

## STATE ROAD DEPARTMENT

OF THE

## STATE OF FLORIDA



FOR THE PERIOD

BEGINNING JANUARY 1, 1925

AND

ENDING DECEMBER 31, 1926

T. J. APPELYARD, INC., TALLAHASSEE, FLORIDA



## PERSONNEL OF DEPARTMENT

---

F. A. Hathaway, Chairman

W. J. Hillman

I. E. Schilling

E. P. Green

J. H. Bayliss

B. A. Meginniss, Attorney

W. P. Bevis, Acting Secretary

---

### PERSONNEL OF EMPLOYEES IN GENERAL CHARGE OF THE WORK OF THE DEPARTMENT

#### *Engineering Division*

J. L. Cresap .....	State Highway Engineer
L. K. Cannon .....	Assistant State Highway Engineer
G. L. Derrick .....	Bridge Engineer
C. W. DeGinther .....	Assistant Bridge Engineer
Harvey A. Hall .....	Testing Engineer
F. W. Berry, Jr. ....	Office Engineer
R. L. Bannerman .....	Division Engineer First Division
J. H. Dowling .....	Division Engineer Second Division
R. J. Cassie .....	Division Engineer Third Division
L. B. Thrasher .....	Division Engineer Fourth Division
A. W. Kinney .....	Division Engineer Fifth Division
R. C. Fergus .....	Division Engineer Sixth Division
Henry Wilson .....	Division Engineer Seventh Division
J. R. Stewart .....	Superintendent of Equipment

#### *Accounting Division*

S. L. Walters .....	Auditor
F. F. Maxwell .....	Traffic Manager
Bettie V. Herring .....	Bookkeeper
H. B. Fryer .....	Bookkeeper
M. P. Relihan .....	Bookkeeper
A. S. Casseaux .....	Bookkeeper

#### *Bureau of Public Roads*

R. E. Toms .....	District Engineer
H. J. Morrison .....	U. S. Highway Engineer
E. R. Epperson .....	Associate Highway Engineer



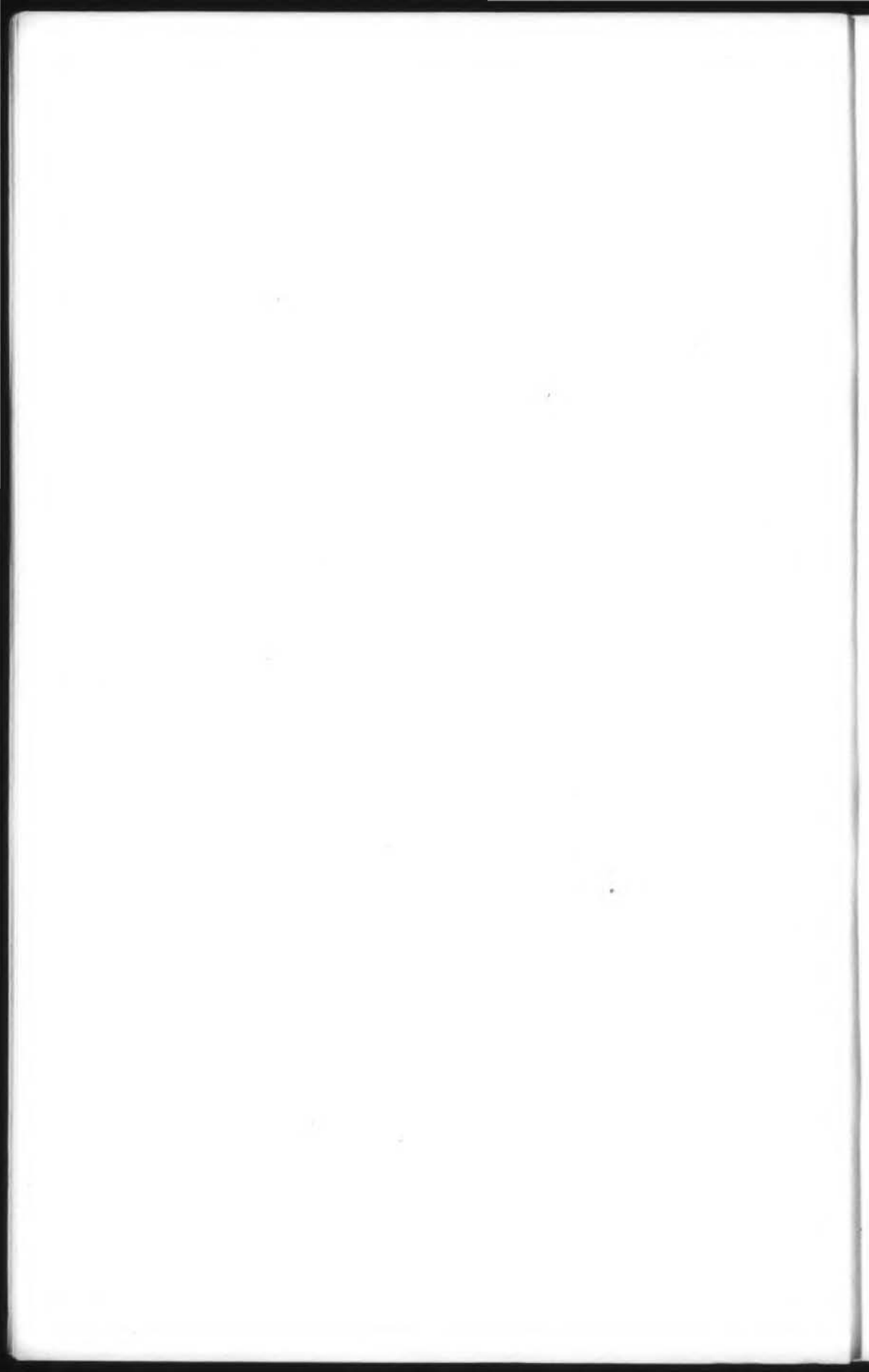




"Way Down Upon De S'wannee Ribber"



Airplane View of F. A. Project 38—Escambia Bay Bridge.



## LETTER OF TRANSMITTAL

---

Hon. John W. Martin, Governor,  
Tallahassee, Florida.

Dear Governor Martin:

In accordance with statutory requirements, we have been pleased to prepare the Biennial Report of the State Road Department, covering the period from January 1, 1925, to December 31, 1926, which I have the honor to transmit herewith.

This report covers the operations of the Department, revenues received, expenditures made and general progress of work. As required by Law, the activities of the Department have been confined to the Preferential roads and on which it will be seen from a study of the report splendid headway has been made.

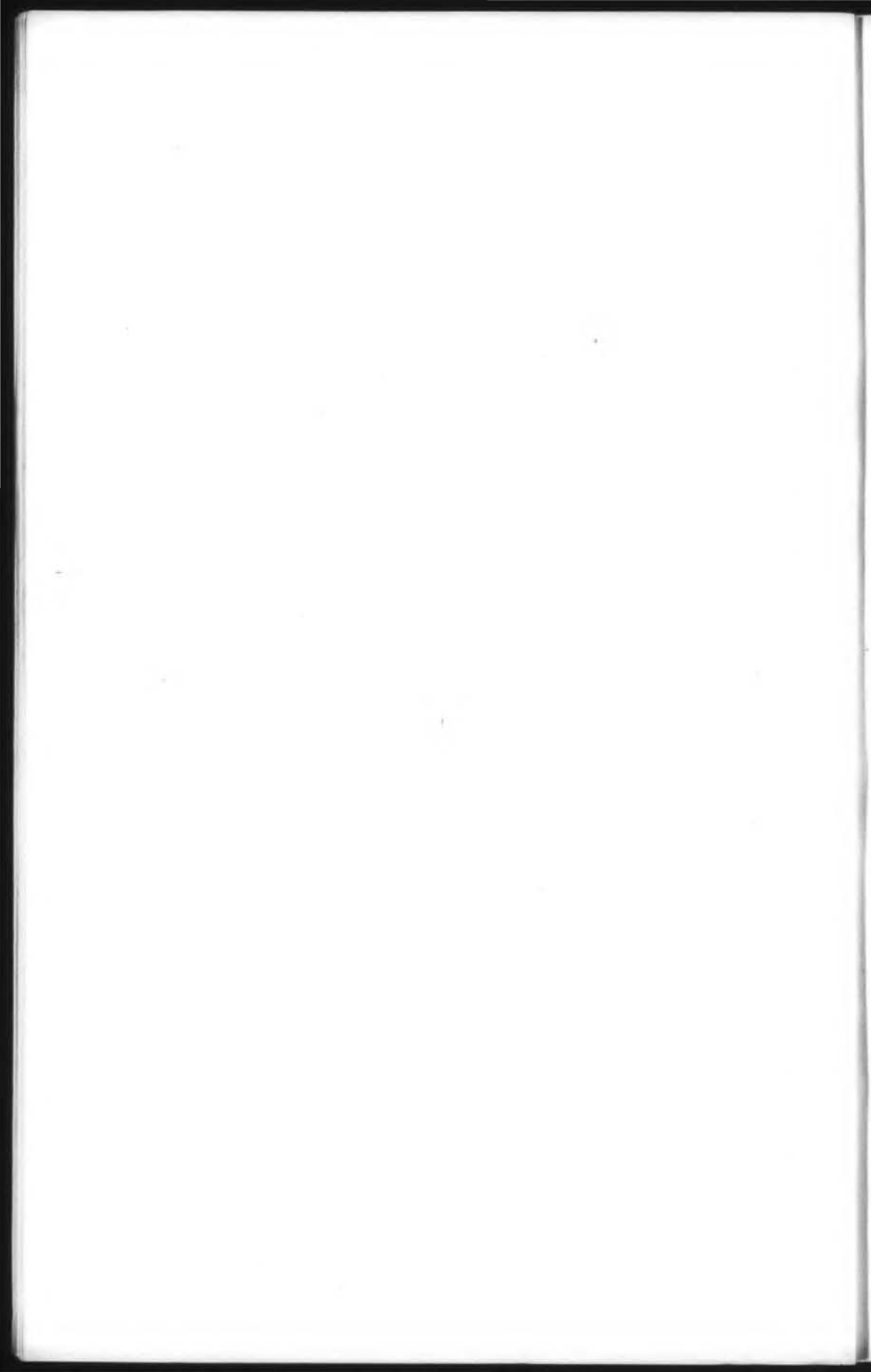
The Department has enjoyed the cooperation of the boards of county commissioners, road trustees and others having official duties to discharge in connection with our work.

We take pride in the achievements of the State Road Department during the period which the within report covers, especially since for the first nine months our progress was blocked because of embargoes on transportation lines, making the delivery of materials next to impossible, the Department being able to do only grading work.

The progress which has been made is due primarily to two things, to organization and to ample funds. Your Excellency and the Legislature of 1925 are due the gratitude of the people of the State for the wisdom and farsightedness which was shown in the enactment of legislation which made these funds possible and available.

Yours respectfully,

F. A. HATHAWAY,  
Chairman.

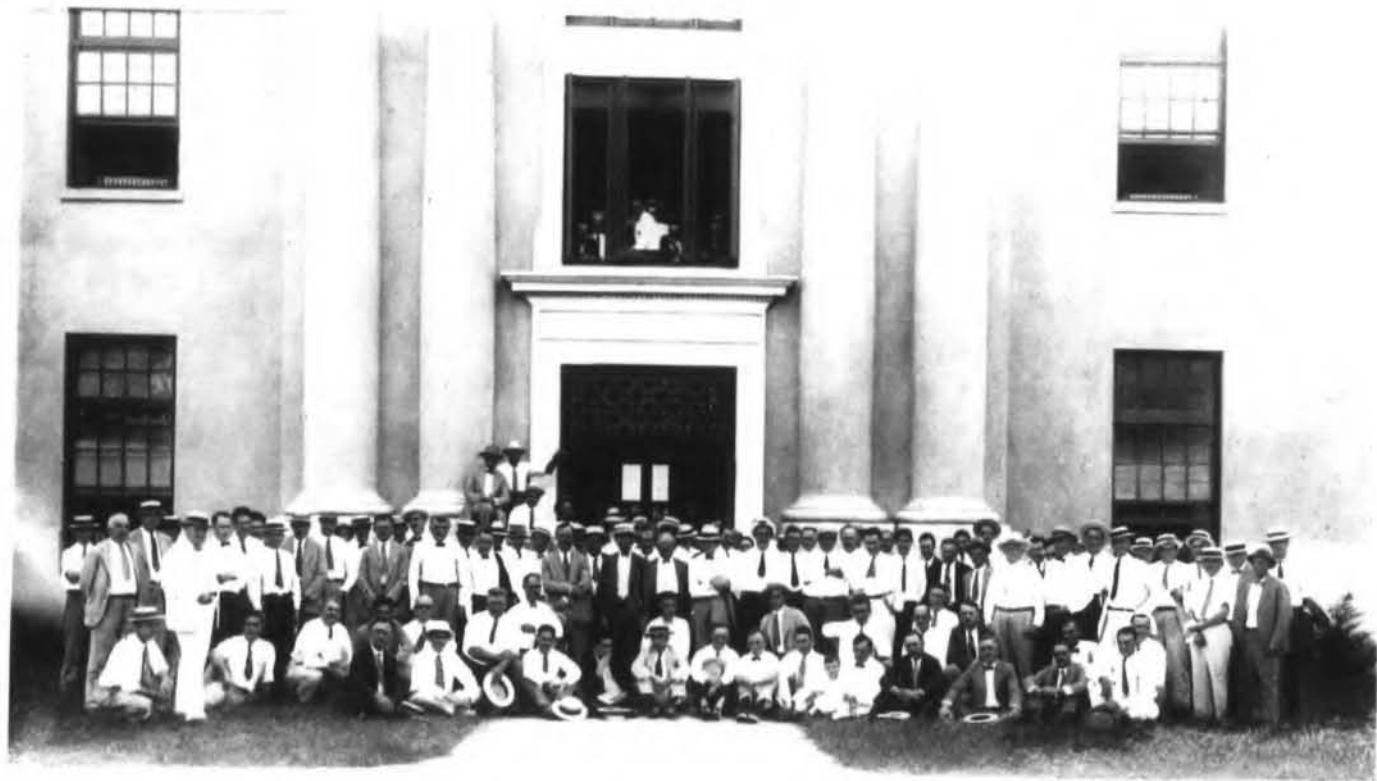




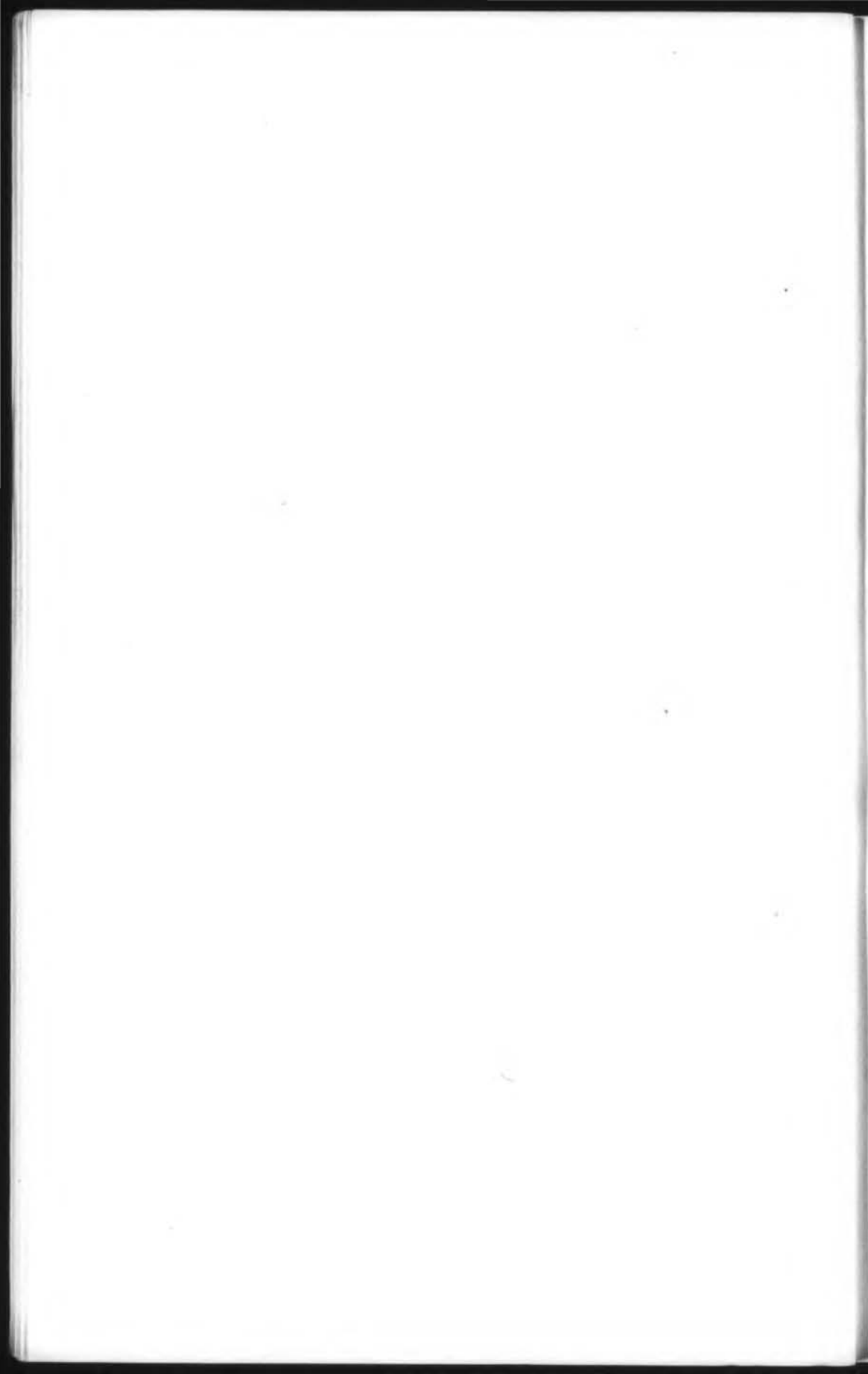
Manatee River Bridge, Road No. 5.







Group photograph of the record-breaking crowd which attended the opening of bids July 21st, 1926, for construction of the greatest number of projects ever advertised for a single letting—15 projects in 12 counties.



# Florida's Road System

---

## CHAPTER 10269—(No. 247).

AN ACT to Amend Section 1 of Chapter 9311, Laws of Florida, approved June 8, 1923, entitled "An Act Declaring, Designating and Establishing a System of State Roads, Providing for the Location Thereof, and Providing that Such Roads when Located and Constructed Shall Become and be the Property of the State."

*Be It Enacted by the Legislature of the State of Florida:*

Section 1. That Section 1 of Chapter 9311, Laws of Florida, Approved June 8, 1923, entitled "An Act declaring, designating and establishing a system of State Roads, providing for the location thereof, and providing that such roads when located and constructed shall become and be the property of the State," be and the same is hereby amended so as to read as follows:

"Section 1. That the following named and numbered roads be and are hereby declared, designated and established as State Roads:

Road No. 1. Extending from the Alabama State Line at Nunez Ferry to Jacksonville, passing through Pensacola, Milton, Crestview, DeFuniak Springs, Bonifay, Chipley, Marianna, Chattahoochee, Quincy, by or near Havana, Tallahassee, Monticello, Greenville, Madison, Live Oak, Lake City, Sanderson, MacClenney and Baldwin.

Road No. 2. Extending from the Georgia State Line north of Jennings to Fort Myers, via Jasper, White Springs, Lake City, High Springs, Gainesville, Ocala, Leesburg, Lisbon, Grand Island, Eustis, Mt. Dora, Plymouth, Apopka, Orlando, Kissimmee, Haines City, Bartow, Arcadia, and Olga Bridge; also from Leesburg to Lakeland, via Mascotte, Groveland, Buck Hill, Polk City, Foxtown

to Lakeland, also from Silver Lake Forks, about four miles east of Leesburg, Lake County, via Tavares to Mount Dora, said extension from Silver Lake Forks via Tavares to Mount Dora to be built with County Funds and with Federal Aid.

Road No. 3. Extending from the Georgia State Line at a point on the St. Mary's River known as Wild's Landing to Orlando, via Jacksonville, Orange Park, Green Cove Springs, Palatka, East Palatka, Crescent City, DeLand and Sanford.

Road No. 4. Extending from the Georgia State Line south of Folkston to Miami, via Hilliard, Callahan, Jacksonville, St. Augustine, Bunnell, Daytona, Titusville, Fort Pierce, West Palm Beach and Fort Lauderdale.

Road No. 4-A. Extending from Miami to Key West via Key Largo, provided, on that portion of the road south of Dade County, Monroe County shall expend \$2,000,000 including the amount already expended on said road and in addition to said \$2,000,000 shall provide one-third of the cost of the bridges from No Name Key to Knights Key and from Grassy Key to Lower Maticumbe.

Road No. 5. Extending from High Springs to Fort Myers, via Newberry, Archer, Williston, Dunnellon, Inverness, Brooksville, Lutz, Tampa, Riverview, Bradenton, Sarasota, Venice and Punta Gorda.

Road No. 5-A is the extension from High Springs to Perry, via Fort White, Branford and Mayo.

Road No. 6. Extending from the Alabama State Line south of Dothan to Port St. Joe, via Campbellton, Marianna, Altha, Blountstown, Scotts Ferry and Wewahatchka.

Road No. 7. From Pensacola to Alabama State Line at Flomaton.

Road No. 8. Extending from Haines City to Fort Pierce via Lake Wales, Frostproof, Avon Park, Sebring, DeSoto City, Lake Annie and Okeechobee City.

Road No. 9. Extending from the Georgia State Line



Scene Along Indian River on the East Coast.



south of Quitman, via Madison, to a point on Road No. 35, near Shady Grove.

Road No. 10. Extending from the Georgia State Line near Beachton to Pensacola, via Tallahassee, Woodville, Newport, and as near St. Marks as practicable, and thence around the coast to Panacea Springs, St. Teresa, Lanark, also via Wakulla, Crawfordville, Sopchoppy, Carrabelle, Apalachicola, Port St. Joe, Panama City, near mouth of Choctawhatchie River, Freeport, Portland, New Valparaiso, Camp Walton, and thence into State Road No. 1 at or near Milton and thence over State Road No. 1 connecting with Pensacola.

Road No. 11. Extending from a point on the Georgia State Line south of Thomasville via Monticello to Capps.

Road No. 12. Extending from the Georgia State Line on the Bainbridge and Quincy Road, via Greensboro, Bristol and to East Point.

Road No. 13. Extending from Cedar Key to Yulee, in Nassau County, via Bronson, Gainesville, Starke, Baldwin and Callahan.

Road No. 14. Extending from a point on Road No. 19, at the Suwannee River Bridge, near Old Town to Hastings on Road No. 4, via Trenton, Newberry, Gainesville and Palatka.

Road No. 15. Extending from a point on Road No. 10, at or near Newport, thence southerly along the gulf as near as practical to St. Petersburg, via or near as practical to the mouths of the Steinhatchee and Suwannee Rivers, Cedar Key, and as near as practical to the mouth of the Crystal River, Homosassa, Weekiwachee Rivers, Aripeka, Hudson, New Port Richey, Tarpon Springs, Clearwater and Largo. And also from Brooksville to Aripeka, via Spring Hill.

Road No. 16. Extending from Ocala to Road No. 5, via Dunnellon, thence to Crystal River.

Road No. 17. Extending from Haines City to Clearwater, via Lakeland, Plant City, Tampa and Oldsmar.

Road No. 18. Extending from Sarasota to Lake Annie, via Arcadia; and Road No. 18-A, extending from a point on Road No. 5, near Bradenton, via Oneco intersecting Road No. 18 at a point about thirty miles east of Sarasota.

Road No. 19. Extending from Tallahassee to Ocala, leaving out of Tallahassee on the St. Augustine Road, thence to or near Chaires via Waukeenah, Capps and Lamont, thence direct to Perry, Cross City, Old Town, Chief-land, Bronson and Williston.

Road No. 20. Extending from Cottondale to Panama City, via Round Lake and from Cottondale north to connect with Road No. 6.

Road No. 21. Extending from Daytona to DeLand.

Road No. 22. Extending from Orlando to Indian River City, via Ft. Christmas, also from Orlando to Crystal River, via Winter Garden, Clermont, Mascotte, Groveland, Bushnell and Inverness.

Road No. 23. Extending from Belleview to Plant City, via Bushnell and Dade City.

Road No. 24. Extending from Kissimmee to Melbourne, via St. Cloud.

Road No. 25. Extending from Olga Bridge to West Palm Beach.

Road No. 26. Extending from a point on Road No. 8 near Lake Annie to Fort Lauderdale, via Moore Haven.

Road No. 27. Extending from Ft. Myers to Miami.

Road No. 28. Extending from Lake City to Bunnell via Palatka, Lulu, Lake Butler, Starke and Keystone Heights.

Road No. 29. Extending from Bithlow to Moore Haven via Holopaw, Kenenville and Okeechobee.

Road No. 30. Also from Frostproof to Vero, via Dougherty Crossing and Yeehaw.

Road No. 31. Road from Ocala to Waldo, via Citra, Island Grove and Hawthorne.

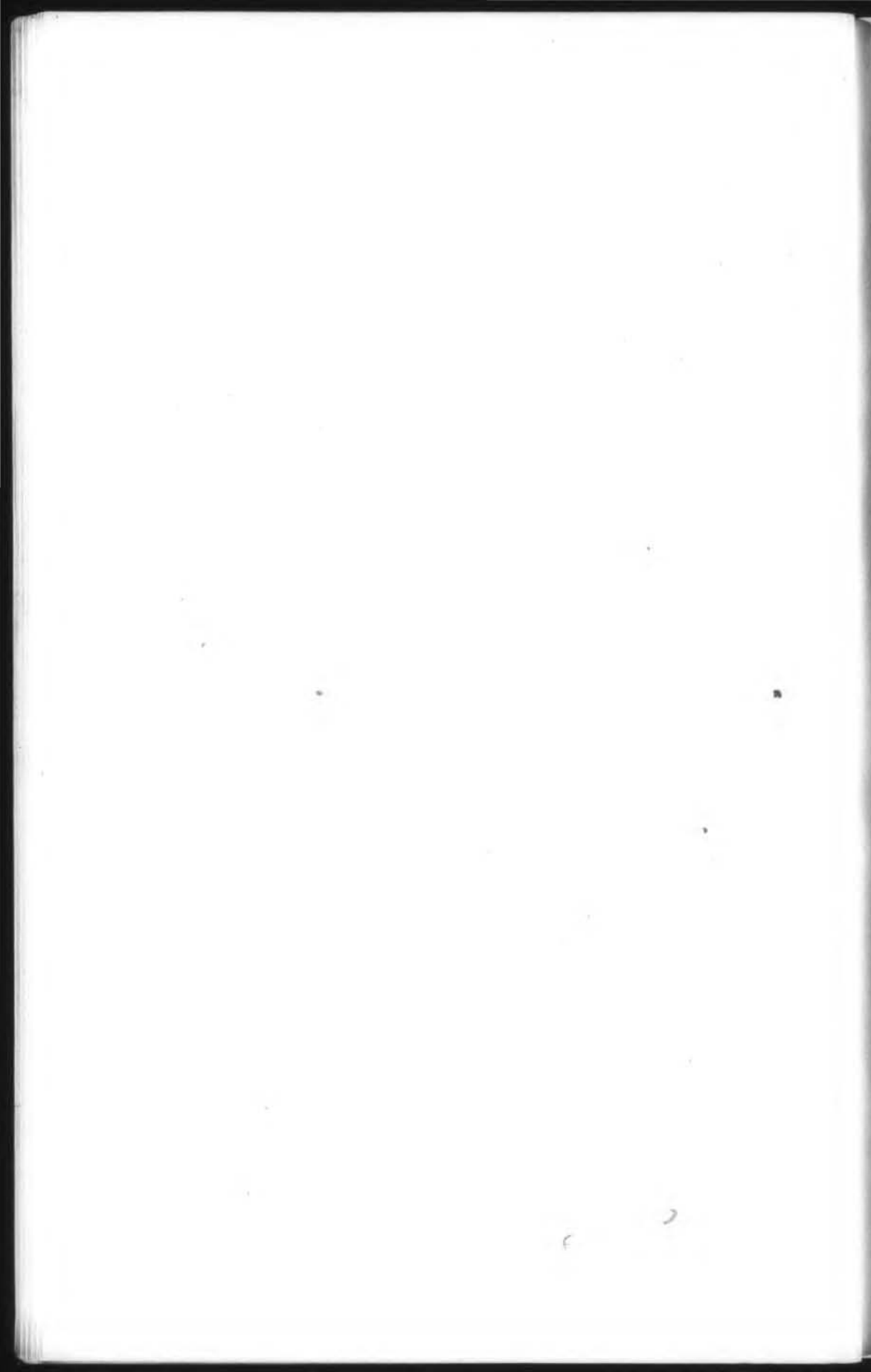
Road No. 32. From Bradenton to Avon Park by way Parrish, Fort Green and Wauchula.

Road No. 33. Extending from the Alabama State Line





Bridge Across Jupiter River.



south of Florala via Laurel Hill to a point on Road No. 1 at or near Crestview.

Road No. 34. Extending from Brooksville to Dade City via Spring Lake and Blanton.

Road No. 35. Extending from the Georgia Line through Ashville and Greenville to a point on Road No. 19.

Road No. 36. Extending from Leesburg to Inverness via Wildwood and Rutland.

Road No. 37. Extending from the Alabama State Line at Dixonville via Allentown, to State Road No. 1 at Milton.

Road No. 38. Extending from Road No. 2 at Weirsdale, east to Umatilla, in Lake County, by way of Stark's Ferry.

Road No. 39. Extending from the Alabama Line through Bonifay to Vernon and thence southerly to Phillips Inlet.

Road No. 40. Extending from the Alabama State Line south of Florala to DeFuniak Springs, and thence southerly to a point on Road No. 10.

Road No. 41. From Milligan, via Baker and Blackman to the Alabama State Line.

Road No. 42. Extending from a point on Road No. 1, north of Aucilla, through Aucilla to Lamont.

Road No. 43. Extending from a point on the Georgia Line southerly through Miccosukee to a point on Road No. 1 and over same easterly to a point north of Lloyd and then southerly through Lloyd and Wacissa to a point on Road No. 15.

Road No. 44. Extending from Sanford to Mims in Brevard County, via Geneva and Southmere.

Road No. 45. Extending from Ocala to Daytona.

Road No. 46. Extending from Alabama State Line near Flomaton, Alabama, to Baker, Florida, via Jay, Berrydale and Munson.

Road No. 47. Commencing at a point on Road No. 4, south of Goodbys Lake in Duval County, running thence southerly as near as practicable along the St. Johns River to a point on Road No. 14 thence to East Palatka.

Road No. 47-A. Palatka to Ocala via Rodman, Orange Springs and Citra.

Road No. 48. Extending from St. Augustine to Starke, via Green Cove Springs.

Road No. 49. Extending from Georgia State Line approximately north of McClenny to Newberry via McClenny, Manning, Sapp, Raiford, Lake Butler, Worthington Springs and Alachua.

Suwannee River Scenic Highway. Extending from Branford to Jasper, via Live Oak.

Road No. 51. Extending from Orlando to Brooksville via Groveland, Riverland and Spring Lake.

Road No. 52. Extending from the Alabama State Line, north of Graceville through Graceville and to the Washington County line north of Chipley, thence through Chipley to bridge across North Bay, near Southport, and extending from Graceville via Jacobs to connect with Road No. 6.

Road No. 53. Extending from Camp Walton along Santa Rosa Sound as near thereto as practicable to Town Point.

Road No. 54. Extending from Crestview by way of Valparaiso to Camp Walton on Road No. 10.

Road No. 55. Extending from a point at or near Pierson on Road Number Three to Road Number Two at Smithwick's Filling Station, in Lake County, through Astor, Umatilla, Eustis, Tavares, Minneola and Clermont.

Road No. 56. Extending from Ellisville to Lake Butler via Providence.

Road No. 57. Extending from New Smyrna to Sanford.

Road No. 58. Extending from the Georgia State Line near Darsey connecting with Road No. 1 at or near Havana.

Road No. 59. Extending from Zolfo Springs via Crewsville, DeSoto City, north side Lake Istakpoga, thence the most acceptable route to a point on State Road No. 8 at or near west end of the Kissimmee River Bridge.



Standard Overhead Crossing.



Road No. 60. Extending from Alabama Line south of Geneva to DeFuniak Springs.

Road No. 61. Extending from Polk City to Auburndale.

Road No. 62. Extending from Road No. 7 on Alabama State Line via Berrydale, Munson, Baker and connecting with Road No. 1 at Milligan.

Provided, that the roads designated as Roads numbered from 1 to 5 inclusive, and Roads No. 8 and 19, said Road No. 19 to be built after Roads No. 1, 2, 3, 4, 5 and 8 have reached that stage of construction that convicts and materials can be transferred on said Road No. 19 as will not interfere or delay the completion of said roads numbered 1, 2, 3, 4, 5, and 8 shall be first completed before any new contracts are made for work on any of the other roads herein provided for, but this proviso shall not be construed to interfere in any way with any contracts already made with any county for construction of any part of any road or roads herein designated, nor with the construction or maintenance of such other roads as have heretofore been designated by the State Road Department and approved by the National Secretary of Agriculture as included in the State and Federal Seven Per Cent Highway System, as follows:

Road No. 6. Alabama Line to Marianna.

Road No. 7. Pensacola to Alabama Line at Flomaton, 44 miles.

Road No. 10. Tallahassee to Georgia Line, 18 miles.

Road No. 13. Baldwin to Gainesville, via Starke, 47 miles.

Road No. 14. Gainesville to Hastings, via Palatka.

Road No. 16. Ocala to Road No. 5, 30 miles.

Road No. 17. Haines City to Tampa, 57 miles.

Road No. 21. Daytona to DeLand, 22 miles.

Road No. 24. Kissimmee to Melbourne, 53 miles.

Road No. 27. Fort Myers to Miami.

Also 45 miles to be designated by the State Road Department and Federal Government as part of the Federal

Seven Per Cent System and such further mileage of roads as may be in the future allowed, allotted and designated by the State Road Department and Federal Government as part of the Federal Seven Per Cent System.

Provided further, that the construction shall begin as soon as possible on roads numbered 5-A, 10, 15 and 20, 28 and that part of Road No. 11, extending from the Georgia State Line to Monticello, and Road No. 47, commencing at a point on Road No. 4 south of Goodley's Lake, in Duval County, running thence southerly as near as practicable along the St. Johns River to a point on Road No. 14, thence to East Palatka, when that stage of construction has been reached on roads 1 to 5 inclusive, and 8 and 19, when labor and equipment may be transferred from roads 1 to 5 inclusive and 8 and 19, that will not delay the construction of said roads 1 to 5 inclusive and 8 and 19.

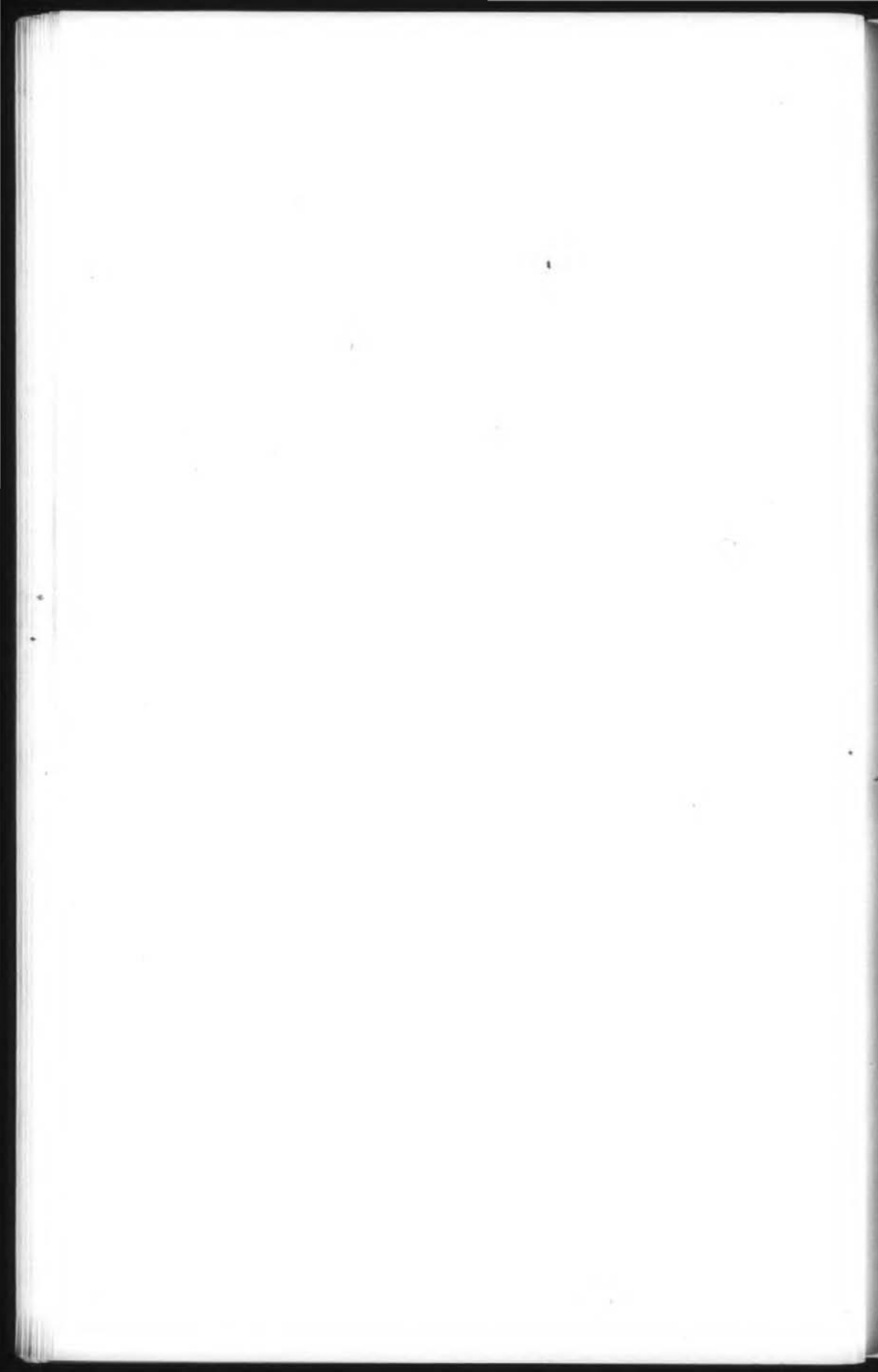
Provided, further, that nothing in this Act shall prohibit or interfere with the State furnishing the engineers and constructing or supervising the construction of any part of said system of state road at any time and in any case, where the county or counties or any road and bridge district, or districts, through which the road or bridge runs and extends, provides all the necessary money, labor and means, including the engineering costs, necessary for the construction thereof.

Provided, further, that the State Road Department shall be and is hereby authorized and empowered to survey and locate the line or route of any State Road or Section of any State Road herein numbered and designated, whenever in the judgment of said Department the doing of such work shall be found to be practicable and to the best interests of the State. Whenever such survey and location shall be made and adopted by the said Department, a map or plat of such survey and location, certified by the Secretary and Chairman of the Department, shall be filed in the Clerk's office of each county, through which said State Road or Section thereof, so surveyed and located shall run.





Grade Separation Crossing Near Ocala.



Sec. 2. This Act shall take effect immediately upon its becoming a law.

Sec. 3. That after the completion of the primary preferential roads as provided in this Act, the State Road Department is hereby authorized to construct any uncompleted portion of Road Number 13.

Approved June 1, 1925.

---

CHAPTER 10270—(No. 248).

AN ACT to Declare, Designate and Establish a Certain State Road.

*Be It Enacted by the Legislature of the State of Florida:*

Section 1. That the following named and numbered road be and is hereby declared, designated and established as a State Road of the System of State Roads of this State with all the rights and considerations of other designated State Roads: ROAD NO. 66, extending from Bristol to Tallahassee, via Telogia, Hosford and Jackson Bluff Bridge.

Sec. 2. This Act shall become a law upon its signature by the Governor.

Approved June 8, 1925.

## CHAPTER 10276—(No. 254).

AN ACT to Designate and Describe the Route of State Road No. 63.

*Be It Enacted by the Legislature of the State of Florida:*

Section 1. That a road beginning at Zolfo Springs, and extending to Bradenton via Ona, in the most direct and practicable route, is hereby declared to be a State road and designated as Road No. 63.

Sec. 2. This Act shall take effect immediately upon its passage and approval by the Governor, or upon becoming a law without such approval.

Approved June 11, 1925.

---

CHAPTER 10186—(No. 164).

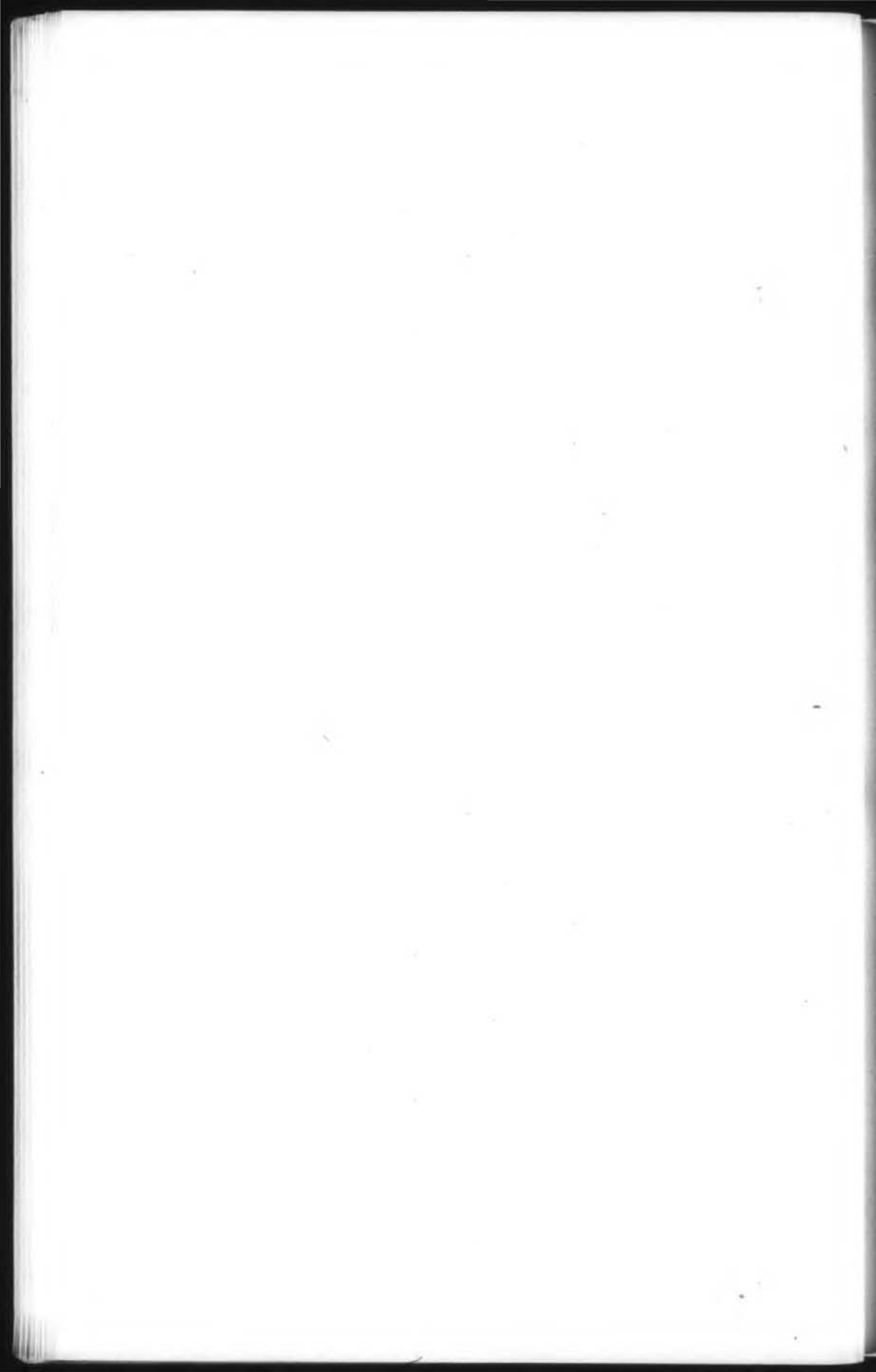
AN ACT to Regulate the Operation of Motor Driven and Other Vehicles on the Public Highways of the State of Florida and to Provide for the Enforcement, and Punishment for the Violation of this Act.

*Be It Enacted by the Legislature of the State of Florida:*

Section 1. No person shall operate a motor vehicle upon the public highway of this State recklessly, or at a rate of speed greater than is reasonable and proper, having regard to the width, traffic, and use of the highway, or so as to endanger the property of the life or limb of any person; Provided, that a rate of speed in excess of twenty-five miles per hour in the residence portion of any city, town or village, and a rate of speed in excess of fifteen miles per hour in the business portion of any city, town or village, and a rate of speed in excess of forty-five miles per hour on any public highway outside of the corporate



Concrete Pavement During Construction.



limits of any incorporated city or town of motor driven vehicles weighing less than five thousand pounds shall be deemed prima facie evidence of reckless driving. That a rate of speed in excess of fifteen miles per hour in the residence portion of a city, town or village, and a rate of speed in excess of ten miles per hour in the business portion of any city, town or village, and a rate of speed in excess of thirty miles per hour on any public highway outside of the corporate limits of any city or town, of motor driven vehicles weighing more than five thousand pounds and less than sixteen thousand pounds shall be deemed a violation of this Section; that a rate of speed in excess of ten miles per hour in the residence portion of any city, town or village, and a rate of speed in excess of eight miles per hour in the business portion of any city, town or village, and a rate of speed in excess of twenty miles per hour on any public highway outside of the corporate limits of any incorporated city or town, of motor driven vehicles weighing more than sixteen thousand pounds shall be deemed a violation of this Section; that the speed of all motor driven vehicles shall be reduced on curves in such manner as to keep the vehicle under the entire control of the driver, provided, further, that no person shall operate upon the public highways inside the incorporate limits of any incorporated city or town of this State a motor vehicle with muffler cut-out open.

Sec. 2. All motor vehicles shall be provided with lights in front and light in the rear. The front or driving lights shall be of the tilting variety or some other device which will kill the glare of the driving lights.

Sec. 3. It shall be unlawful to stop any motor vehicle on the public roads, for either convenience or repair, but in all cases where possible to do so shall turn off the road to the right and the left wheel nearest the center of the paving shall not be more than one foot on the side of the paving. All vehicles shall drive on the right side of the road except when passing a slower vehicle.

Sec. 4. It shall be unlawful to operate upon any hard surfaced road in Florida any log cart, tractor, well machine or any steel tired vehicle other than the ordinary farm wagon or buggy, or any other vehicle or machine that is likely to damage a hard-surfaced road except ordinary wear and tear on the same.

Sec. 5. It shall be unlawful for any officer in the discharge of his duties, as provided for in this Act, to demand an excessive appearance bond and in all such cases the gravity of the offense committed shall be considered in the requirement of such appearance bonds.

Sec. 6. It shall be unlawful for any city or town to pass or attempt to enforce any ordinance in conflict with the provisions of this Act. Provided, however, that this Act shall not apply to school zones.

Sec. 7. Any person convicted of the violation of any of the provisions of this Act shall be guilty of a misdemeanor and upon conviction shall be punished by a fine of not more than one hundred (\$100.00) dollars or by imprisonment for not more than ninety (90) days, or by both such fine and imprisonment.

Sec. 8. All laws and parts of laws in conflict herewith are hereby repealed.

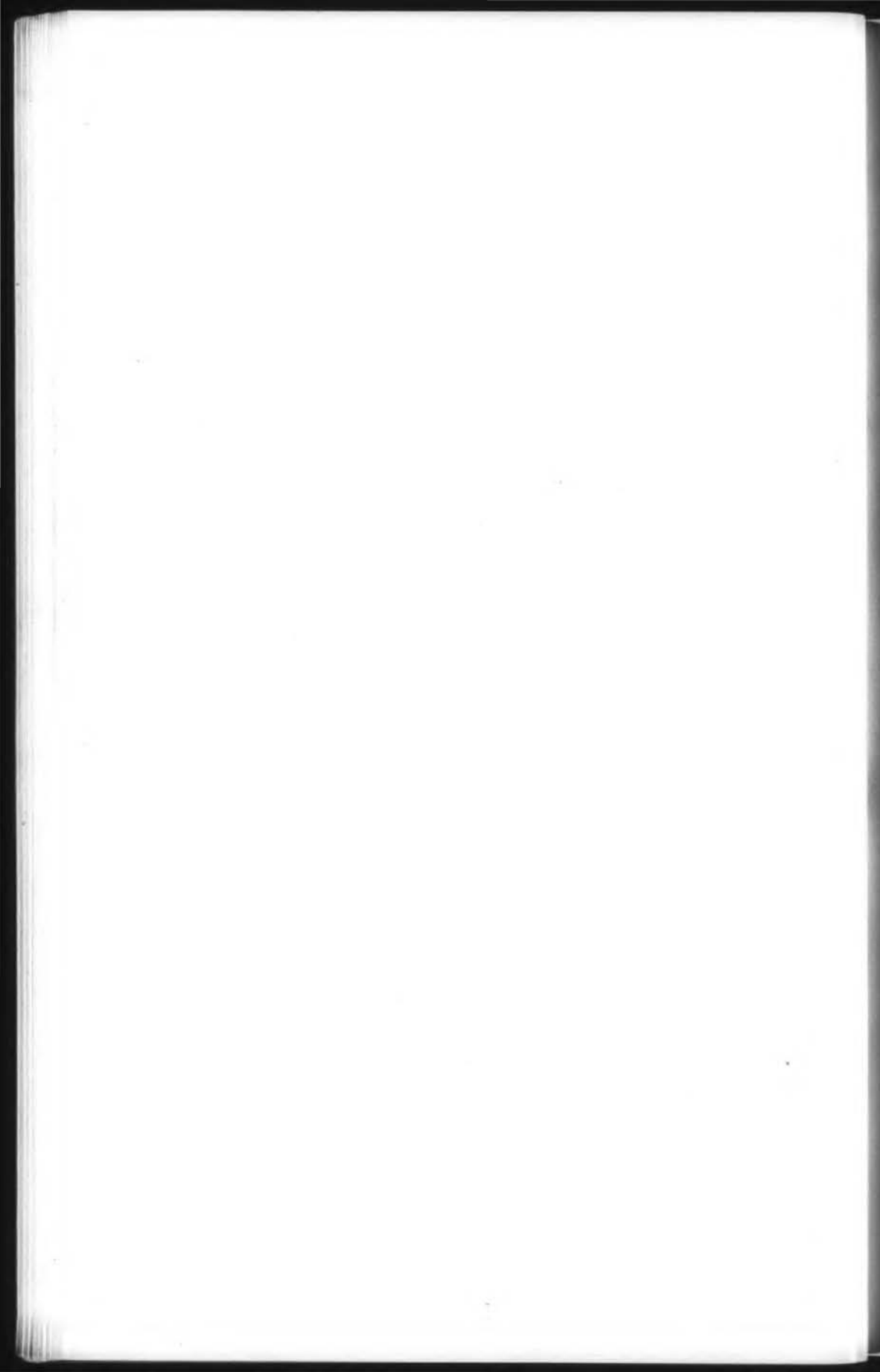
Sec. 9. This Act shall take effect upon its becoming a law.

Approved June 8, 1925.





Plain Cement Concrete Construction. Project 565. Road 1.



RULES AND REGULATIONS OF THE SECRETARY  
OF AGRICULTURE FOR CARRYING OUT THE  
FEDERAL HIGHWAY ACT (EXCEPT THE PRO-  
VISIONS THEREOF RELATIVE TO FOREST  
ROADS).

REGULATION 1.—DEFINITIONS

Section 1. For the purposes of these regulations, the following terms shall be construed, respectively, to mean:

*Act.*—The act of Congress approved July 11, 1916, entitled "An act to provide that the United States shall aid the States, in the construction of rural post roads, and for other purposes" (30 Stat. 355), as amended by the act of Congress approved February 28, 1919, entitled "An act making appropriations for the service of the Post Office Department for the fiscal year ending June 30, 1920, and for other purposes" (40 Stat. 1200, 1201), and as amended by the act of Congress approved November 9, 1921, entitled "An act to amend the act entitled 'An act to provide that the United States shall aid the States in the construction of rural post roads, and for other purposes,' approved July 11, 1916, as amended and supplemented, and for other purposes" (Public, No. 87, 67th Cong.).

*Secretary.*—The Secretary of Agriculture of the United States.

*Bureau of Public Roads.*—The Bureau of Public Roads of the United States Department of Agriculture.

*Authorized representatives of the Secretary.*—The Chief of the Bureau of Public Roads and such other officials and employees thereof as he may designate from time to time.

*Federal aid highway system.*—The system of Federal aid highways, established by actual agreement and joint action of the States and the Secretary of Agriculture, and upon which all Federal aid funds shall be spent.

*Primary highways.*—The primary or interstate portion of the Federal aid highway system composed of not to exceed three-sevenths thereof.

*Secondary highways.*—The secondary or intercounty portion of the Federal aid highway system consisting of at least four-sevenths thereof.

*Ten per cent fund.*—Items for engineering, inspection, and unforeseen contingencies not exceeding 10 per cent of the total estimated cost of the construction.

#### REGULATION 2.—APPLICATION OF REGULATIONS

Section 1. These regulations apply to all provisions of the act, except the provisions thereof relative to forest roads and trails, unless hereafter so made applicable by order of the Secretary.

Sec. 2. These regulations shall apply as fully where the extent to which the State may engage in road construction and maintenance work, or raise State revenues therefor, is limited by its existing constitution and laws as in any other case.

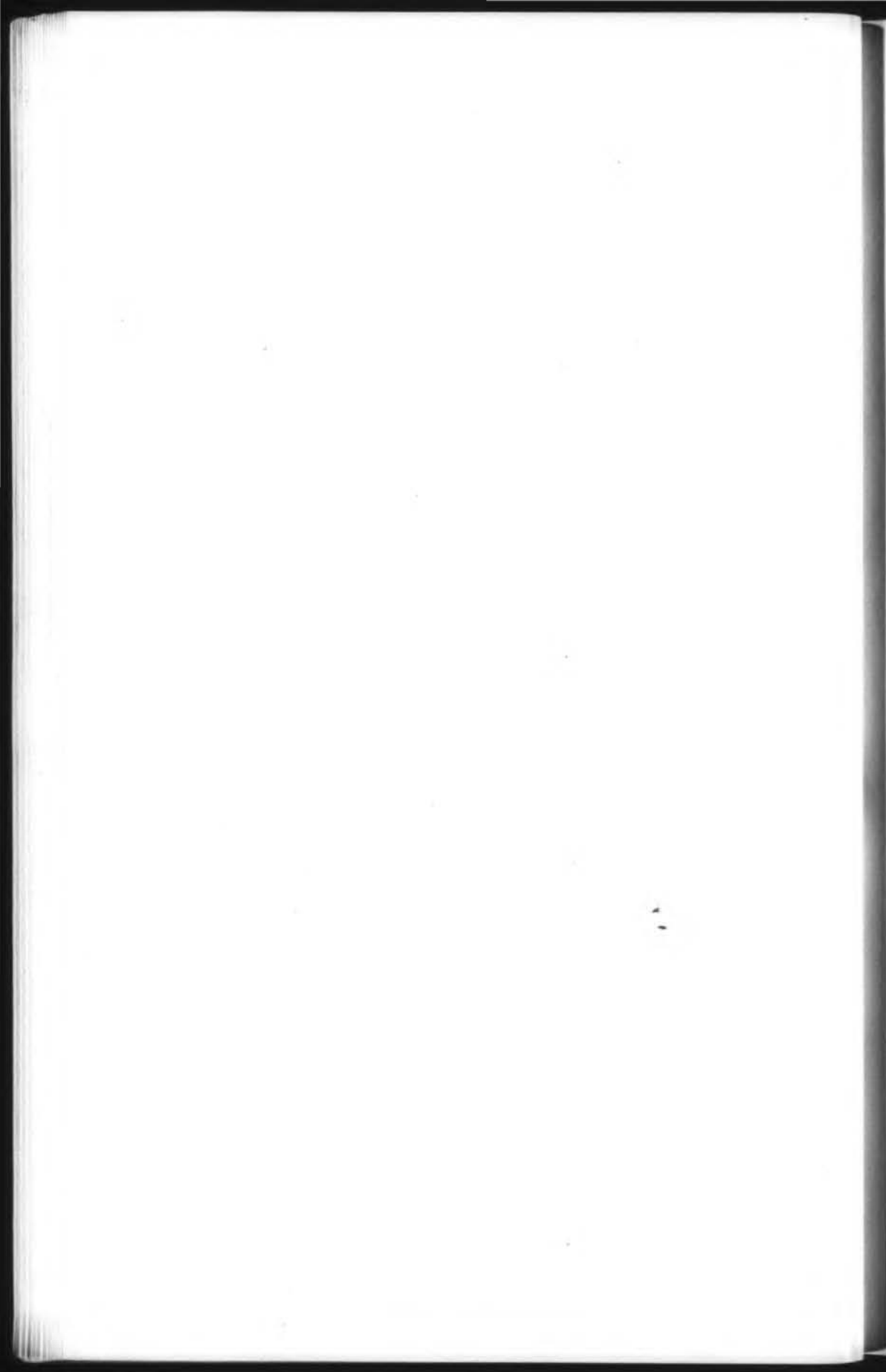
#### REGULATION 3.—INFORMATION FOR THE SECRETARY

Section 1. Before any agreement is made upon any road or roads to be constructed in a State, or as to the character and method of construction, there shall be furnished the Secretary upon his request, by or on behalf of the State, general information as to its laws affecting roads and the authority of the State and local officials in reference to the construction and maintenance of roads; as to the State highway department, how equipped and organized; as to the existing provisions of its constitution or laws relative to State revenues for the construction, reconstruction, or maintenance of roads; as to funds that will be available to meet the State's share of the cost of the construction work to be performed and the general source of such funds; and as to provisions made, or to be made, for maintaining roads upon which Federal aid funds will be expended.

Sec. 2. Information requested by the Secretary or his authorized representatives relating to the maintenance of



Concrete Pavement. Road 7. Escambia County.



roads constructed under the provisions of the act shall be furnished, from time to time, by the State highway departments, on forms supplied by the Bureau of Public Roads.

Sec. 3. Data furnished by or on behalf of a State shall be supplemented by such reports of the Bureau of Public Roads as the Secretary may from time to time require.

#### REGULATION 4.—FEDERAL AID HIGHWAY SYSTEM MAP

Section 1. Each State highway department shall file with the Secretary of Agriculture a State map showing the proposed Federal aid highway system and indicating the primary and the secondary portions thereof, in such form and with such information as he may require.

Sec. 2. The Secretary, through his authorized representatives, will make an examination of the proposed system and will from time to time notify the State highway department of the acceptability of the parts of the system examined.

Sec. 3. When agreement has been reached between the State highway department and the Secretary as to the whole (or if the State so desires, of a material portion) of the Federal aid highway system, the State shall make formal request for the approval of the Secretary of Agriculture. This request will be accompanied by a State map showing the full proposed Federal aid highway system with the primary and secondary highways upon which formal approval is requested, in such form and with such information as may be prescribed by the Secretary or his authorized representatives.

Sec. 4. Pending the formal approval of the State highway system in whole or in part by the Secretary of Agriculture, only such projects will be approved as are on routes indicated on the proposed Federal aid highway system as submitted under section 1 and tentatively accepted by the Secretary under section 2 of this regulation: *Provided*, That the Secretary of Agriculture may approve project statements submitted by the State highway depart-

ments prior to the selection, designation, and approval of the system of Federal aid highways herein provided for if he may reasonably anticipate that the projects will become a part of such system.

#### REGULATION 5.—PROJECT STATEMENTS

Section 1. A project statement may be submitted for the whole or any part of a continuous route or routes embraced in the Federal aid highway system selected or designated in accordance with the provisions of the act, preference being given to such projects as will expedite the completion of a system of highways interstate in character.

Sec. 2. Prior to the selection, designation, and approval of the Federal aid highway system, project statements may be submitted for any route or part of a route if the Secretary may reasonably anticipate that such route will become a part of such system. After the Federal aid highway system shall have been selected, designated, and approved no project statements shall be submitted for any route or part of a route not embraced in the system so selected, designated and approved.

Sec. 3. A project statement shall contain such information as the Secretary may require to be submitted on a form supplied by the Bureau of Public Roads and shall be accompanied by a sketch map in sufficient detail and covering such length of road as may be necessary to determine the fitness of the location as a part of the Federal aid highway system and with the termini of the proposed improvement indicated thereon.

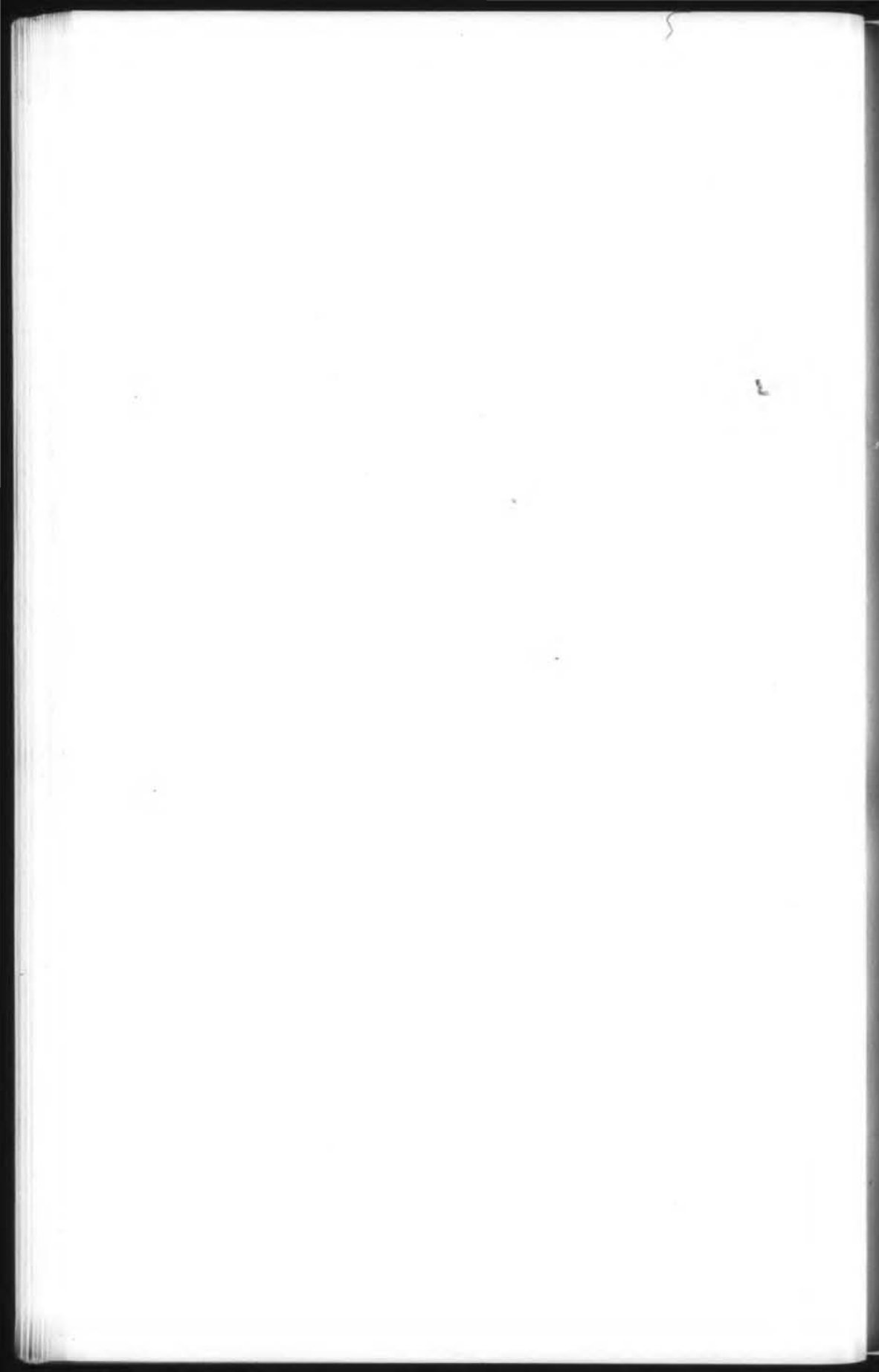
#### REGULATION 6.—SURVEYS, PLANS, SPECIFICATIONS, AND ESTIMATES

Section 1. The surveys, plans, specifications and estimates shall show in convenient form and detail the work to be performed, and the probable cost thereof, all in con-





Project 421. St. Mary's River Bridge. Road 3.



formity with the standards, governing form, and arrangement prescribed by the Secretary.

Sec. 2. Copies of the specifications shall be submitted with the plans and estimates, except that where standard specifications have been approved by the Chief of the Bureau of Public Roads a statement to the effect that approved standard specifications govern may be submitted in lieu of the printed documents.

Sec. 3. Until plans, specifications, and estimates for a project or part thereof have been submitted and found satisfactory for recommendation, and the State has been so notified by the district engineer of the Bureau of Public Roads, no project or part thereof shall be let to contract.

Sec. 4. The estimate for each project shall show the estimated quantity and the estimated cost of each item of construction in detail and, separately, the 10 per cent fund, and shall not include any expense for advertising.

Sec. 5. Unless State standard contract and bond forms have been approved there shall be submitted with each set of plans for the approval of the Secretary copies of the form of contract together with all documents referred to therein or made a part thereof, and of the contractor's bond which it is proposed to use on the project. No alteration of either of these forms, when once approved, shall be made until it is approved by the Secretary.

Sec. 6. Where any part of the cost of a project is to be furnished by a county or other local subdivision or subdivisions of a State, the plans, specifications, and estimates shall be accompanied by certified copy of each resolution or order, if any, of the appropriate local officials, or such other showing as the Secretary may require respecting the funds which are made available, or respecting the supervision of the construction of the road and of the control of the money provided for paying such cost.

Sec. 7. Right of way ample for any project shall be provided and no incidental damages to adjoining property, due to construction work paid for by or on behalf of the

State, shall be included in the estimate or be paid in any part, directly or indirectly, by the Federal Government.

Sec. 8. Grade crossings occurring on the Federal aid highway system shall be classified for priority of improvement by agreement between the State highway departments and the Bureau of Public Roads.

Sec. 9. No part of the expense of making surveys, plans, specifications, or estimates, by or on behalf of the State prior to the beginning of construction work, shall be included in the estimate or paid by the Federal Government.

Sec. 10. Subsequent to the execution of the agreement no change which will increase the cost of a project to the Federal Government shall be made, except upon approval by the Secretary of Agriculture, and no changes shall be made in the termini or type, except upon approval of the Chief of the Bureau of Public Roads, but minor alterations which do not affect the general nature of the improvement or increase the total cost to the Federal Government may be authorized by the Chief of the Bureau of Public Roads or his authorized representative.

#### REGULATION 7.—PROJECT AGREEMENTS

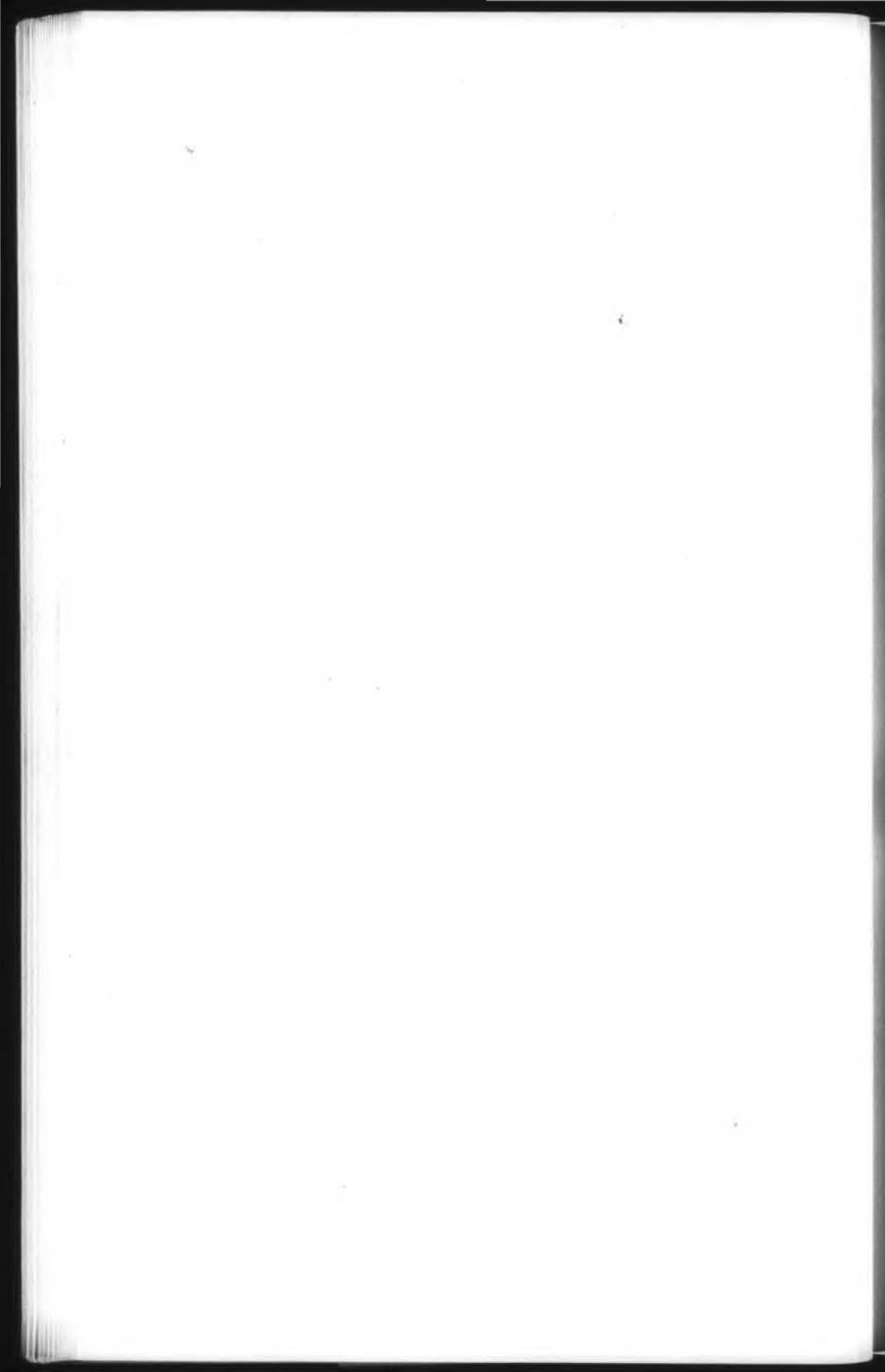
Section 1. A project agreement between the State highway department and the Secretary shall be executed in triplicate on a form furnished by the Secretary. No payment shall be made by the United States unless or until such agreement has been executed, nor on account of work done prior to recommendation by the district engineer of the Bureau of Public Roads that the plans, specifications, and estimates be approved.

#### REGULATION 8.—CONTRACTS

Section 1. No part of the Federal money set aside on account of any project shall be paid until it has been shown to the satisfaction of the Secretary that adequate methods, either advertising or other devices appropriate for the pur-



Concrete Pavement on Road No. 4.



pose, were employed, prior to the beginning of construction, to insure economy and efficiency in the expenditure of such money.

Sec. 2. Upon publication of advertisements copies thereof shall be furnished to the Bureau of Public Roads.

Sec. 3. Bids shall conform to the standard proposal form, and the items shall be the same as those contained in the estimate provided for in Regulation 6, section 4.

Sec. 4. Copy of the tabulated bid prices, showing the unit prices and the totals of each bid for every project, shall be furnished promptly to the Bureau of Public Roads.

Sec. 5. In advance of the acceptance of any bid sufficient notice of the time and place the contract is to be awarded shall be given to the Bureau of Public Roads to enable it, if it so desires, to have a representative present. When a bid has been accepted prompt notice thereof shall be given to the Bureau of Public Roads.

Sec. 6. If the contract be awarded to any other than the lowest responsible bidder the Federal Government shall not pay more than its pro rata share of the lowest responsible bid, unless it be satisfactorily shown that it was advantageous to the work to accept the higher bid.

Sec. 7. The specifications and plans shall be made a part of the contract.

Sec. 8. A copy of each contract, as executed, shall be promptly certified by the State highway department and furnished to the Secretary and no alteration in the contract shall be subsequently made without the approval of the Secretary.

#### REGULATION 9.—CONSTRUCTION

Section 1. Suitable samples of materials to be used in construction work shall be submitted, by or on behalf of the State highway department, to the Bureau of Public Roads whenever requested.

Sec. 2. Unless otherwise stipulated in writing by the Secretary or his authorized representative, materials for

the construction of any project shall be tested, prior to use, for conformity with specifications, according to methods prescribed or approved by the Bureau of Public Roads.

Sec. 3. No part of the money apportioned under the act shall be used, directly or indirectly, to pay or to reimburse a State, county, or local subdivision for the payment of any premium or royalty on any patented or proprietary material, specification, process, or type of construction unless purchased or obtained on open actual competitive bidding at the same or a less cost than unpatented articles or methods, if any, equally suitable for the same purpose.

Sec. 4. The supervision of each project by the State highway department shall include adequate and continuous engineering inspection throughout the course of construction.

Sec. 5. Written notice of commencement and completion of work on any project shall be given promptly by the State highway department to the Bureau of Public Roads.

Sec. 6. Reports of the progress of construction, showing force employed and work done, shall be furnished as requested by the Secretary or his authorized representatives.

#### REGULATION 10.—RECORDS AND COST KEEPING

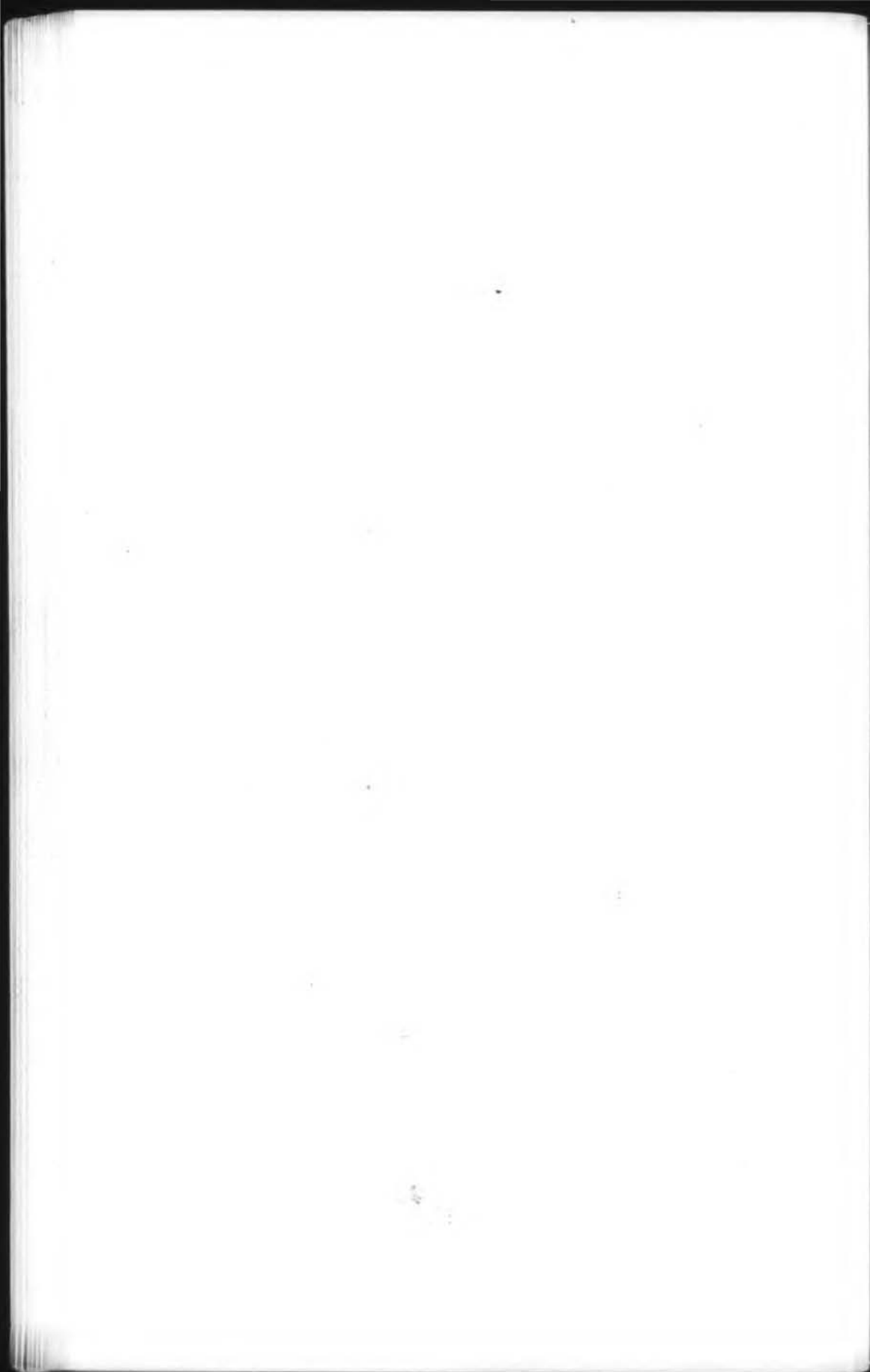
Section 1. Such records of the cost of construction, of inspection, of tests, and of maintenance, done by or on behalf of the State, shall be kept, by or under the direction of the State highway department, as will enable the State to report, upon the request of the Secretary or his authorized representatives, the amount and nature of the expenditure for these purposes.

Sec. 2. The accounts and records, together with all supporting documents, shall be open at all times to the inspection of the Secretary or his authorized representatives, and copies thereof shall be furnished when requested.





Project 545. Road 5. Hernando County. Sheet Asphalt.



## REGULATION 11.—PAYMENTS

Section 1. Vouchers, in the form provided by the Secretary and certified as therein prescribed, showing amounts expended on any project and the amount claimed to be due from the Federal Government on account thereof, shall be submitted by the State highway department to the Bureau of Public Roads, either after completion of construction of the project, or, if the Secretary has determined to make payments as the construction progresses, at intervals of not less than one month.

## REGULATION 12.—SUBMISSION OF DOCUMENTS

Section 1. Papers and documents required by the act or these regulations to be submitted to the Secretary may be delivered to the Chief of the Bureau of Public Roads or his authorized representatives and, from the date of such delivery, shall be deemed submitted.

## RAILROAD AGREEMENT.

This Agreement, made and entered into by and between the State Road Department of Florida, party of the first part, and Atlantic Coast Line Railroad Company, Seaboard Air Line Railway Company, Florida East Coast Railway Company and Louisville & Nashville Railroad Company, parties of the second part, witnesseth:

Whereas, said party of the first part is carrying on throughout the State of Florida a large program of highway improvement and construction; and

Whereas, the parties of the second part desire to co-operate with the party of the first part in the elimination as far as possible of grade crossing accidents; and

Whereas, the parties hereto recognize that these accidents usually occur on the main traveled highways; therefore,

It is agreed by and between the parties hereto as follows:

The parties hereto will co-operate with each other in the establishment, construction, relocation and repair of highway crossings under the jurisdiction of said State Road Department upon the following terms and conditions:

## (1)

Whenever a State road crosses an existing railroad right-of-way, the State Road Department shall have the right to determine whether such State road and railroad shall cross each other at grade, or by over-pass or under-pass, under the terms and conditions of this agreement.

The State Road Department may under this agreement provide for the elimination of existing grade crossings of State roads over existing railroad tracks.

## (2)

When it is provided that an over-pass crossing of a highway over an existing railroad shall be established, either



F. A. Project 38—Escambia Bridge. View of 200-ft. Steel Swing Span. This bridge was opened to traffic July 8th and stretches three and one-half miles across Escambia Bay.



as a new project or as the improvement of an existing crossing, the railroad involved shall be responsible for one-half of the entire cost of the bridge which shall span the opening over the tracks of the railroad from abutment to abutment and including such abutments; and the State Road Department shall be responsible for the balance of the cost. The railroad company involved shall maintain the supporting abutments and bents of the bridge which shall span the opening over the tracks of the railroad from abutment to abutment; and the State Road Department shall maintain the balance of the structure. When an over-pass is constructed, it shall be so constructed as to be sufficient to accommodate at least two railway tracks; this requirement may be waived and construction for one railway track provided for at the option of the railway company.

## (3)

When it is provided that an under-pass crossing of a highway under an existing railroad shall be established, either as a new project or as the improvement of an existing crossing, the railroad involved shall be responsible for one-half of the expense of all excavations through the existing railroad fill of the railroad right-of-way and one-half of the complete cost of the structure necessary to carry the railroad tracks on the railroad right-of-way including the foundation, sub-structure and super-structure; and the State Road Department shall be responsible for the balance of the cost. The railroad company involved shall maintain the abutments and super-structure of such under-pass and the State Road Department shall maintain the roadway and drainage thereof. When an under-pass is constructed, it shall be so constructed as to be sufficient to accommodate at least two railway tracks; this requirement may be waived and construction sufficient for one railway track provided for at the option of the railway company.

## (4)

When it is provided that a crossing at grade shall be established over a railroad, either as a new project or as a re-location of an existing crossing, the railroad company involved shall be responsible for one-half of the cost of such crossing between points ten (10) feet beyond the end of the cross ties on each side of its tracks; and the State Road Department shall be responsible for the balance of the cost. The railroad company involved shall maintain the crossing between points ten (10) feet beyond the end of the cross ties on each side of its tracks and the State Road Department shall maintain the balance of such crossing.

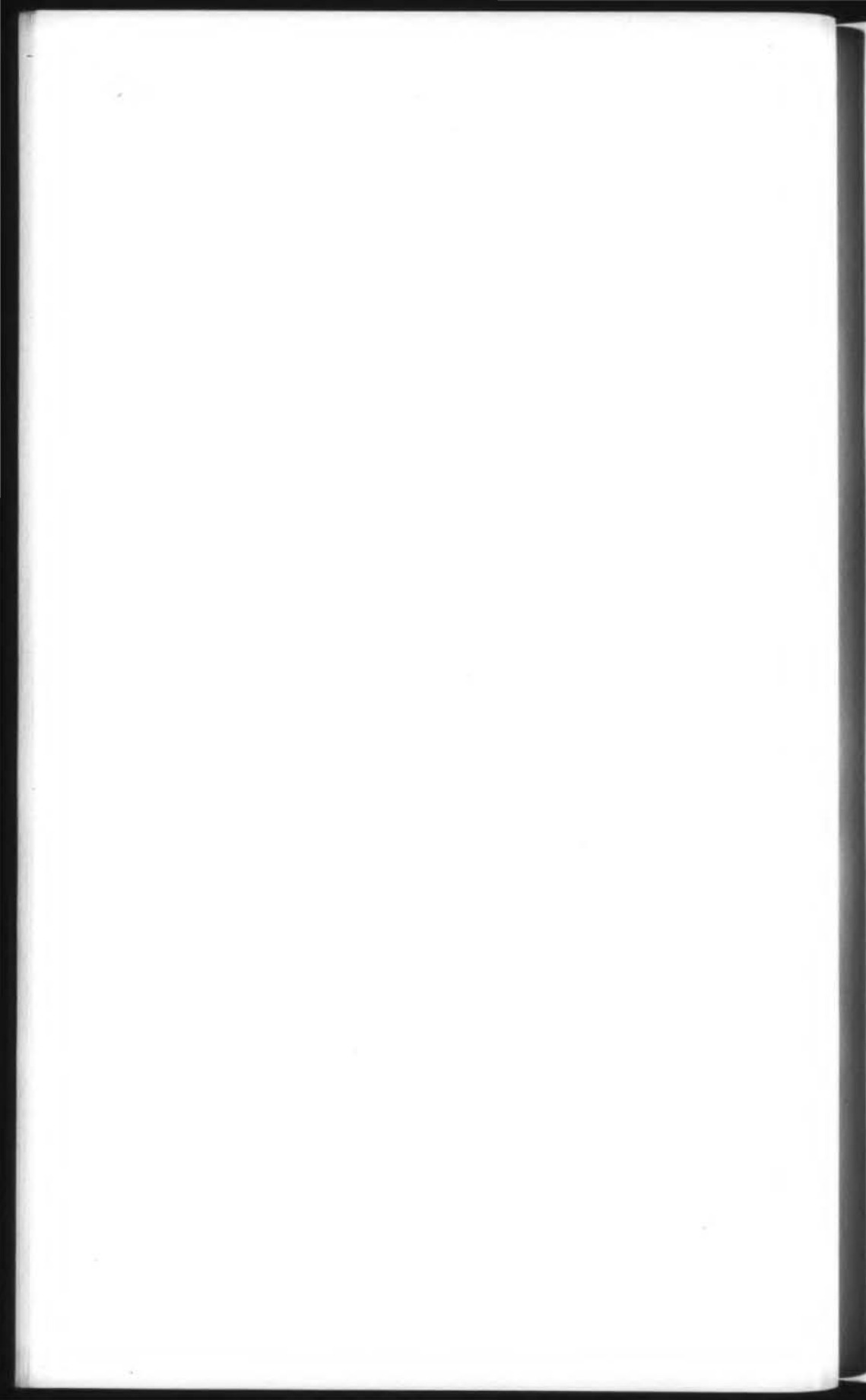
## (5)

Where a new line of railroad, owned or controlled by the parties hereto, crosses an existing highway under the jurisdiction of the State Road Department, unless said State Road Department and the railroad shall agree for a crossing at grade (in which event section four shall apply), the railroad shall have the right to elect whether said highway shall be carried over or under the proposed railroad track. In case it elects that the highway shall be carried over the railroad then the State Road Department shall bear one-half of the cost of the bridge spanning the railroad tracks, from abutment to abutment, including abutments, and the railroad shall provide the necessary fills to restore the existing width of the highway road-bed for the highway approaching said bridge, which shall be on a grade of five per cent unless otherwise agreed upon, and shall co-operate with the State Road Department in the reconstruction of the pavement disturbed to an amount equal to the appraised value of the existing pavement so disturbed. In case the railroad shall elect that the railroad shall be carried over the highway, then and in that event the State Road Department shall pay one-half of the cost of the railroad bridge spanning the highway, including the abutments





Project 597. Road 4. Volusia County South of Oak Hill.



thereto, and the railroad shall provide such fills for its tracks approaching said bridge as may be necessary.

The same policy is to prevail as to elimination of grade crossings over new railroad construction as is applied in the case of old railroad construction; that is, the elimination of a grade crossing over new railroad construction will be required only under the same circumstances and conditions as would cause the elimination of a grade crossing over old construction, and there shall be no discrimination as between the two types of construction.

The construction of additional track or tracks on an existing right-of-way of an existing railroad shall not be considered new construction, but crossing of highways under the jurisdiction of the State Road Department over such multiple track lines shall be governed by the provisions of Sections one, two, three and four hereof.

(6)

No railroad shall be obligated under this proposal and its acceptance to expend in any one calendar year a sum in excess of one-fifth of one per cent of its total gross operating revenue derived from business in the State of Florida both interstate and intrastate for the calendar year next preceding; provided, that this clause shall not preclude consideration of any crossing improvement by any railroad company or companies under this proposal and the participation of such company or companies in such improvement provided the exigencies of the case and the circumstances of the company or companies in the judgment of its officers warrant such improvement.

(7)

The agreement shall be effective from and after this, the 3rd, day of July, A. D. 1924.

(8)

Whenever an overhead bridge, under-pass, or grade crossing, is substituted for an existing grade crossing, the existing grade crossing shall be eliminated, closed and vacated.

(9)

This agreement shall remain of force until terminated by one year's written notice from either party unto the other.

STATE ROAD DEPARTMENT OF FLORIDA.

By H. B. Phillips, Chairman.

Attest: Ella Cleary Thompson, Secretary.

ATLANTIC COAST LINE RAILROAD COMPANY.

By Lyman DeLano, Executive Vice-President.

SEABOARD AIR LINE RAILWAY COMPANY.

By W. L. Stanley, Vice-President.

FLORIDA EAST COAST RAILWAY COMPANY.

By H. N. Rodenbaugh, General Manager.

LOUISVILLE & NASHVILLE RAILROAD CO.

By W. L. Mapother, President. '

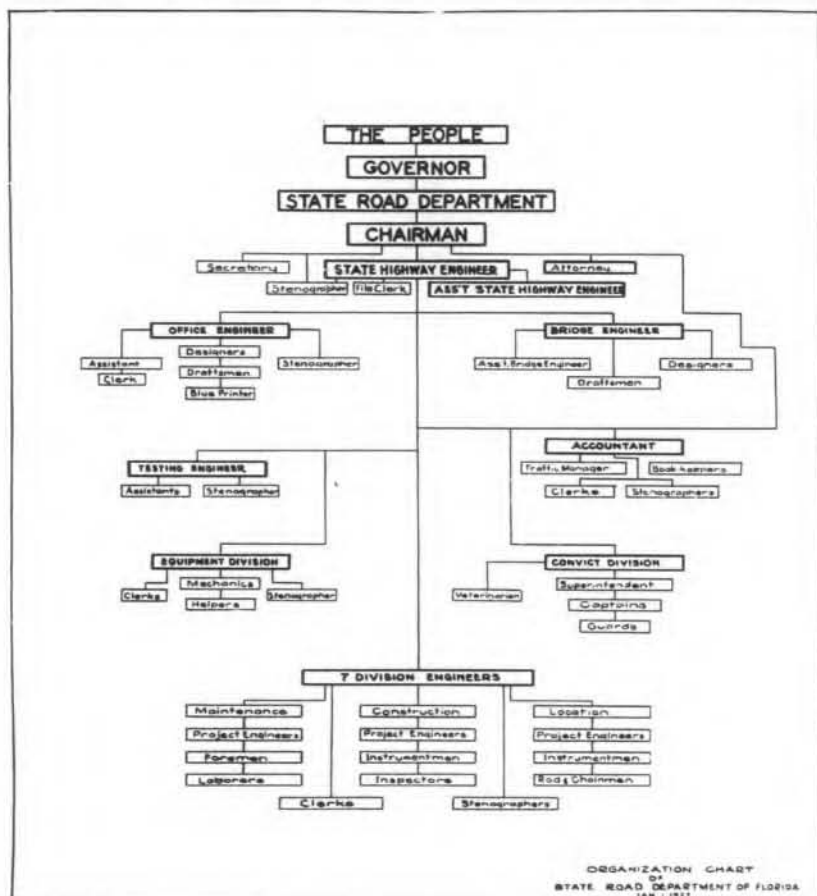


F. A. Project 39-A. Road 4. Sebastian River Bridge.

# ORGANIZATION CHART

## OF

### State Road Department of Florida



# State Highway Engineer's Report

---

Tallahassee, Florida.

Hon. Fons A. Hathaway, Chairman,  
State Road Department,  
Tallahassee, Florida.

Dear Sir:

I have the honor to submit to you the report of the State Highway Engineer for the period from January 1st, 1925, to December 31st, 1926.

Respectfully submitted,

J. L. CRESAP,  
State Highway Engineer.

## ORGANIZATION

The Legislature of Florida at its 1923 session passed a law defining the organization of the State Road Department, as well as that of the employees and fixed the salaries of these. The Department is composed of five members, one of which is appointed from each of the four Congressional districts, and one from the State at large. The members select one of their number as chairman, who is the executive head of the Department. The Department is empowered to employ assistants for the purpose of prosecuting the work. These employees consist of a Secretary, State Highway Engineer, Assistant State Highway Engineer, Division Engineers, Testing Engineer, Office Engineer, Bridge Engineer, Auditor, Superintendent of Equipment, and other assistants necessary for the proper prosecution of the work.

## SURVEYS

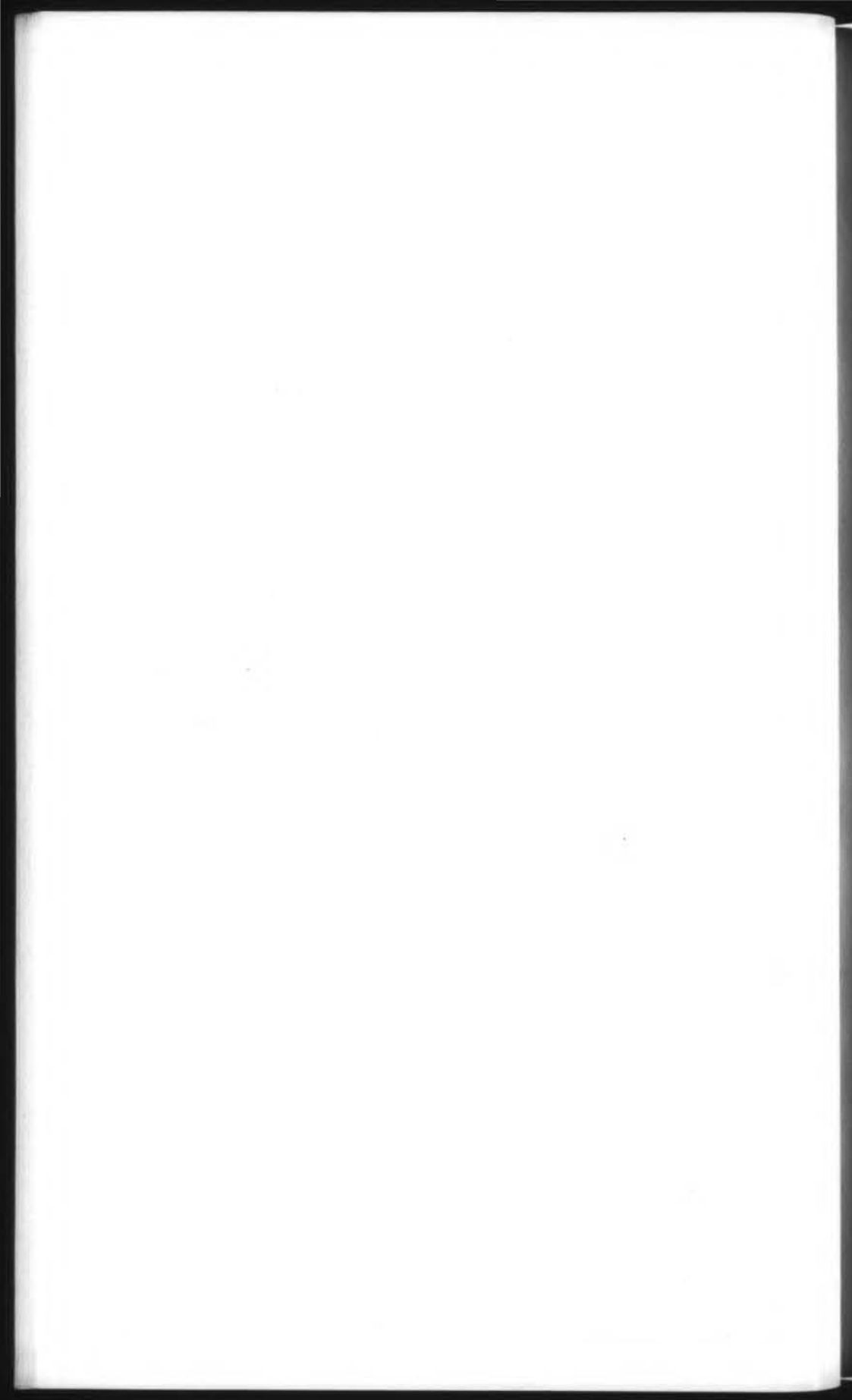
Although the Legislature has laid out a very definite system of roads on which State funds may be spent on construction, yet this Department was authorized or directed to make surveys of any roads or sections of roads on the State system, when the same would facilitate the work done by the several counties. During the past two years the Department has not only made the necessary surveys on the Preferential system, but has made numerous surveys at the request of various counties, that had funds to expend on State roads, not in the preferred class. This Department has not only made such surveys, but has prepared detail plans governing the construction of these roads.

A great deal of care is used in location of all the highways, as it is the desire of the Department to make them as safe as possible, as well as to select the most economical line on which the highway may be constructed. Taking into consideration the matter of safety, will take two examples of main highways, these roads Nos. 1 and 4. On Road 1, by using care and judgment in location between Jacksonville and Pensacola twenty-two railroad crossings have been eliminated by relocation, eight by constructing overhead crossings and three by building underpasses, thereby doing away with the hazards attached to thirty-three grade crossings. On State Road No. 4 there have been eliminated by relocation twenty-four grade crossings, and two by constructing overhead crossings. These are only a few of the grade crossings that have been done away with in the construction of trunk line roads to date.





Project 505. Road 2. Columbia County. Rock base, Surface Treated.



STATE ROAD DEPARTMENT OF FLORIDA  
SURVEYS AND LOCATIONS MADE 1925-1926

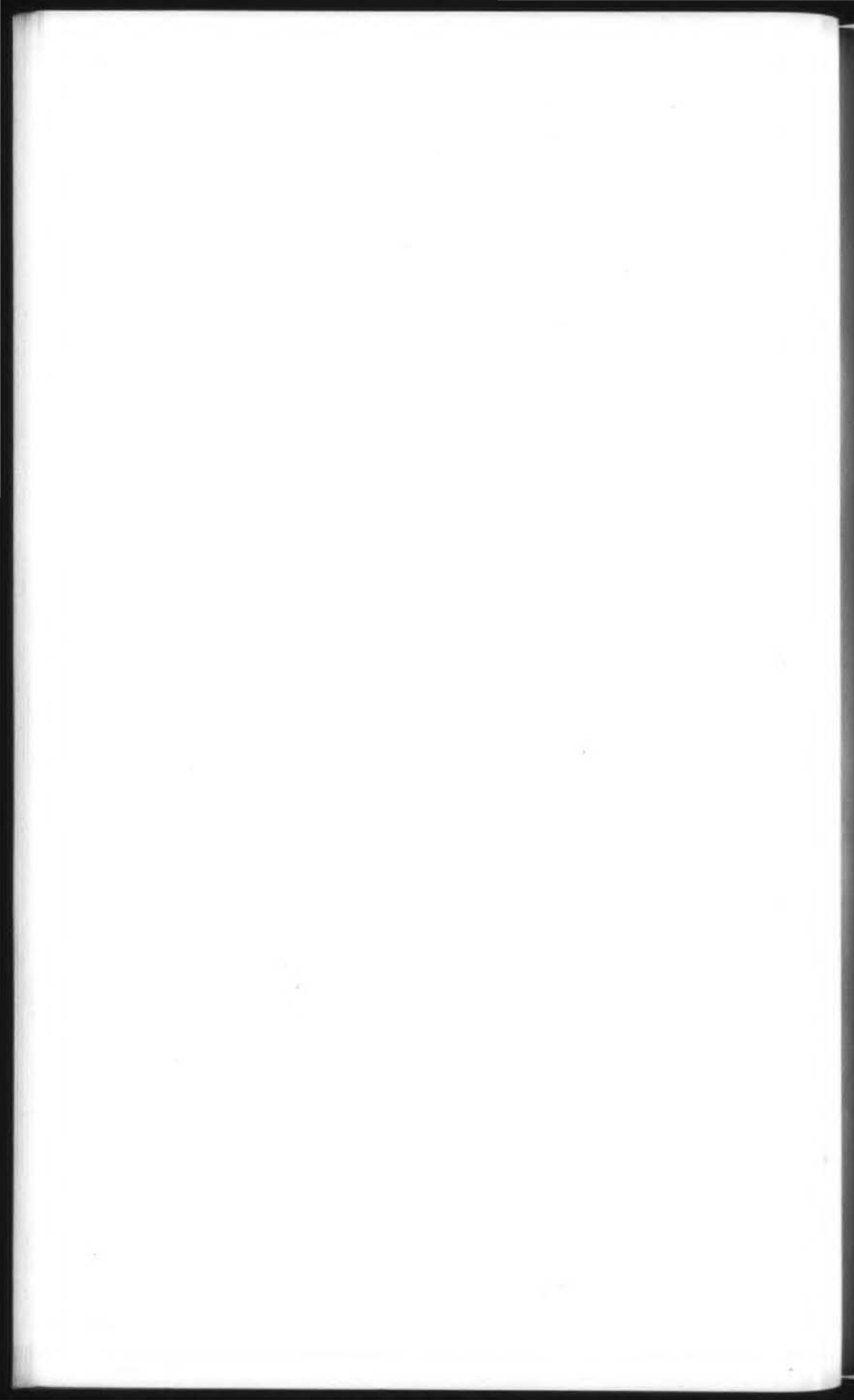
Road No.	Project No.	County	Length
1	6	Madison	5.45
2	7	Hamilton	13.00
2	19	Hamilton	8.20
2	31	Hamilton	11.86
4	41	Dade	11.76
4	46	Nassau	11.51
4	47	St. Johns	14.96
4	48	St. Johns	15.93
4	49	Flagler	13.82
14	50-A	Putnam	6.18
14	50-B	Putnam	9.79
14	50-C	Putnam	10.03
7	51	Escambia	6.30
1	52	Escambia	10.08
2	53-A	Lake	7.09
2	53-C	Lake	1.87
1	54	Leon	19.23
14	55	Alachua	16.77
10	56	Leon	17.98
3	57	Nassau	3.50
1	58	Jefferson	6.31
1	59	Jefferson	9.10
20	500-A	Bay	9.65
20	500-B	Bay	10.00
6	502	Gulf	6.94
1	504	Columbia	9.14
1	529	Suwannee	12.90
1	533	Suwannee	13.39
3	543	Seminole	14.20
8	562-A	Highlands	5.37
5	564-B	Charlotte	9.73
5	564-C	Charlotte	3.93
1	571	Madison	14.26
2	573-A	Orange	17.92
2	573-B	Orange	16.80
9	574	Madison	11.66
19	580	Dixie	17.10
5-A	587	Columbia	4.37
5	589	Lee	8.16
1	590	Santa Rosa	9.00
10	592	Franklin	10.21
5	614	Sarasota	19.58
5	615	Sarasota	11.53
1	621	Okaloosa	17.35
3	627	Putnam	6.27
1	631	Holmes	8.52
4	640-A	Martin	9.00

STATE ROAD DEPARTMENT OF FLORIDA  
SURVEYS AND LOCATIONS MADE 1925-1926—(Continued)

Road No.	Project No.	County	Length
4	640-B	Martin .....	11.80
4	641	Palm Beach .....	13.79
3	642	Putnam .....	10.82
10	644-A	Wakulla .....	7.35
10	645	Franklin .....	6.68
8	647	Highlands .....	9.00
2	648	Hardee .....	10.02
33	649	Okaloosa .....	10.43
10	651	Gulf .....	14.71
33	652	Okaloosa .....	11.50
4	653	Broward .....	13.46
4	654	Broward .....	12.33
18	655	Highlands .....	15.00
1	658	Washington .....	8.20
3	659	Clay .....	13.26
5	663	Citrus .....	8.03
6	666	Jackson .....	6.52
4	668	Brevard .....	13.45
27	669	Dade-Collier .....	120.00
6	670	Jackson .....	6.01
20	671	Jackson .....	10.00
1	672	Leon .....	9.92
1	673	Gadsden .....	9.00
1	674	Duval .....	3.50
17	675	Polk .....	5.16
19	676-A	Levy .....	9.95
19	676-B	Levy .....	13.14
19	676-C	Levy .....	11.09
13	677-A	Levy .....	6.95
13	677-B	Levy .....	11.58
13	677-C	Levy .....	10.16
13	677-D	Levy .....	7.58
10	678	Bay .....	9.00
5	679	Hernando .....	7.11
5	682	Citrus .....	6.50
4	683-C	Palm Beach .....	8.27
10	685	Franklin .....	17.23
2	687-A	Lake .....	15.03
2	687-B	Lake .....	15.22
5-A	689	Alachua .....	1.67
12	690	Gadsden .....	14.00
4	691	Indian River .....	7.50
4	692	St. Lucie .....	8.93
4	693	St. Lucie .....	12.73
4	694	Martin .....	4.48
2	695	Lake .....	10.50
19	698	Leon .....	12.45



Concrete Pavement. State Road No. 1.



STATE ROAD DEPARTMENT OF FLORIDA  
SURVEYS AND LOCATIONS MADE 1925-1926—(Continued)

Road No.	Project No.	County	Length
19	699	Jefferson .....	7.70
19	700	Jefferson .....	9.00
10	702	Franklin .....	8.00
10	703	Franklin .....	6.00
3	704	Orange .....	3.88
28	706	Putnam .....	20.10
43	707	Leon .....	8.50
11	708	Jefferson .....	7.98
39	709	Holmes .....	15.00
17	710	Hillsboro .....	24.65
10	712	Wakulla .....	6.00
28	713	Columbia .....	10.00
5-A	718	Columbia .....	8.22
11	720	Jefferson .....	9.58
43	722	Jefferson .....	8.90
66	723	Leon .....	11.80
66	724	Leon .....	10.67
10	725	Wakulla .....	6.00
10	728	Leon .....	11.67
17	732	Polk .....	9.89
33	733	Walton .....	4.23
39	736	Holmes .....	3.50
10	740	Gulf .....	9.00
13	742	Alachua .....	7.65
35	747	Jefferson .....	6.51
14	749	Gilchrist .....	6.74
1	565	Madison .....	15.99
Total Miles .....			1345.87

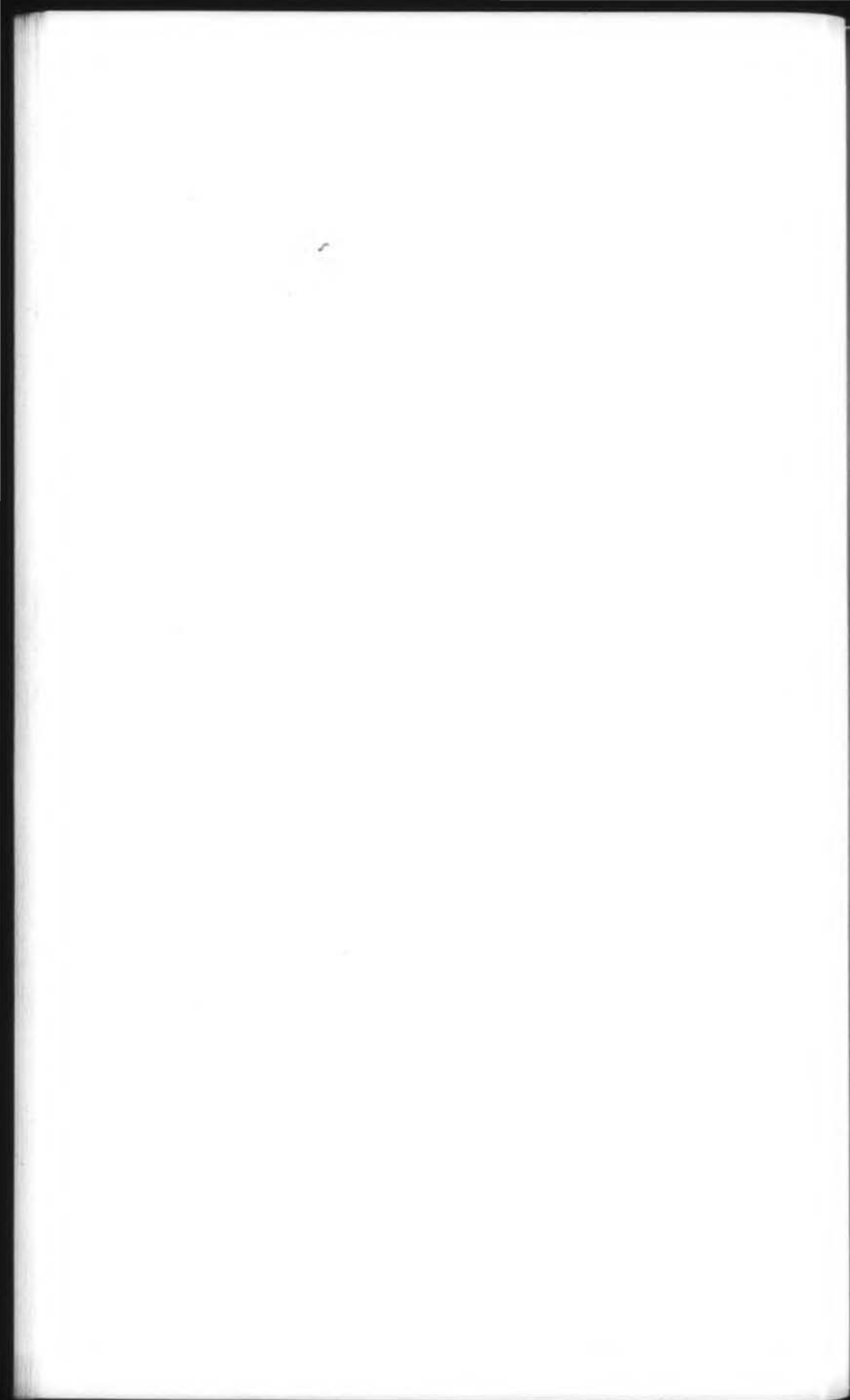
## PLANS

The making of plans is one of the very essential items in connection with the construction of highways. The Department has arranged so that all plans for all highways built by the Department are made in the Tallahassee office. Surveys having been made, all other data which pertains to the topography of the country, soil conditions as well as soundings for all bridges is secured and furnished for the guidance of the drafting force in making up the plans. The Department not only makes plans for such highways as it is authorized by the Legislature to construct, but also makes plans for lateral or county highways which are a part of the State system of roads, when requested to by the counties, when such counties have made provision by bond issue or otherwise for their construction. This is in accordance with a law passed by the Legislature for the purpose of rendering aid to the counties in their road work.





Seminole Bridge in Pinellas County.



STATE ROAD DEPARTMENT OF FLORIDA  
PLANS COMPLETED 1925-1926.

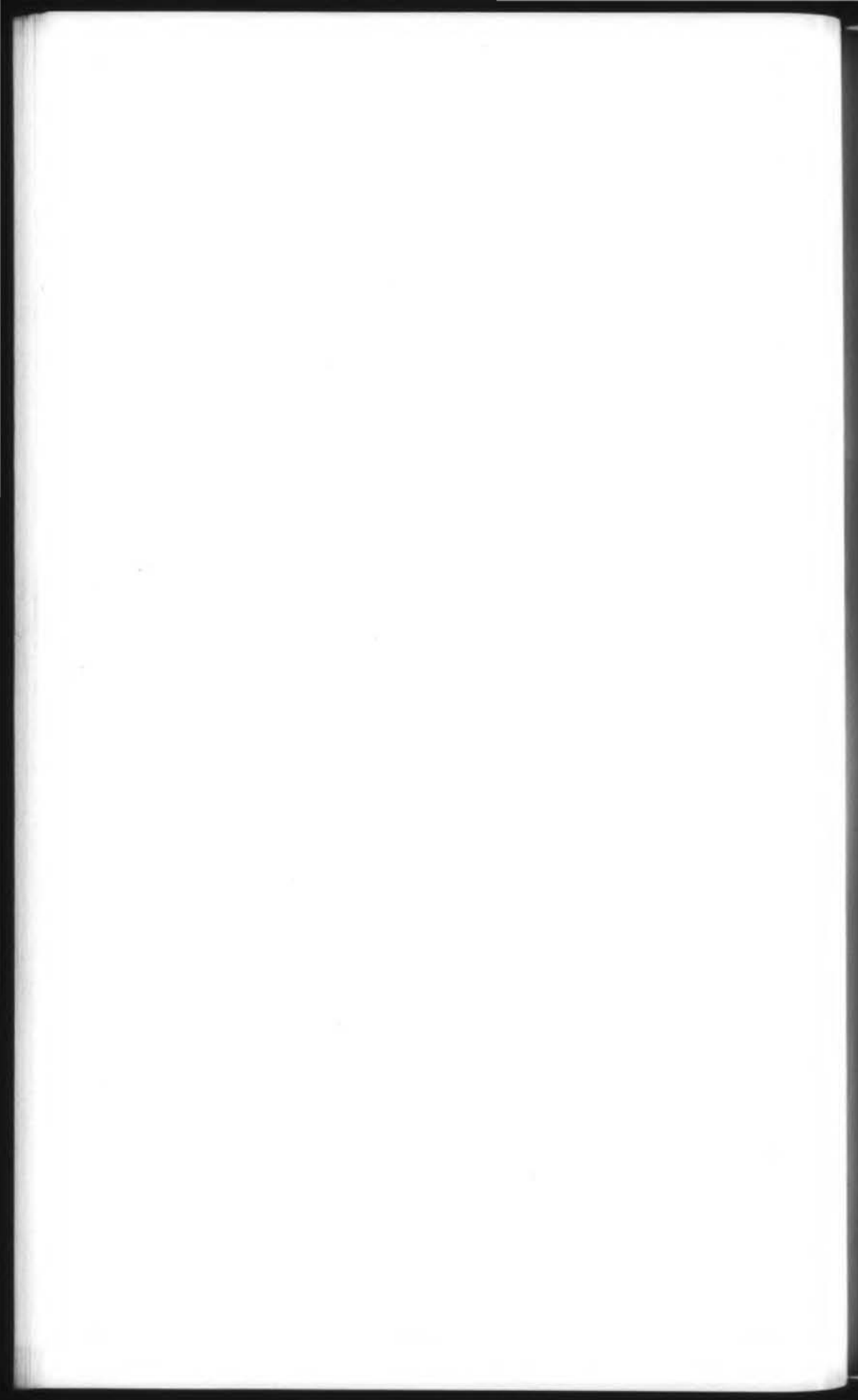
Road No.	Project No.	County	Length
2	7	Hamilton .....	13.00
2	19	Hamilton .....	8.20
2	31	Hamilton .....	11.86
4	41	Dade .....	11.76
4	46	Nassau .....	11.51
4	47	St. Johns .....	29.92
4	48	St. Johns .....	15.93
4	49	Flagler .....	13.82
14	50-A	Putnam .....	6.18
14	50-B	Putnam .....	9.79
14	50-C	Putnam .....	10.03
7	51	Escambia .....	6.30
1	52	Escambia .....	10.08
2	53-A	Lake .....	13.34
2	53-C	Lake .....	1.87
1	54	Leon .....	9.23
14	55	Alachua .....	16.77
10	56	Leon .....	17.98
1	58	Jefferson .....	6.31
1	59	Jefferson .....	9.10
20	500-A	Bay .....	9.65
1	504	Columbia .....	9.14
1	529	Suwannee .....	12.90
1	533	Suwannee .....	13.39
3	543	Seminole .....	16.20
8	562-A	Highlands .....	5.37
5	564-B	Charlotte .....	9.73
5	564-C	Charlotte .....	3.93
1	565	Madison .....	15.99
1	571	Madison .....	14.26
2	573-A	Orange .....	17.92
2	573-B	Orange .....	16.80
19	580	Dixie .....	17.10
5-A	587	Columbia .....	4.37
5	589	Lee .....	8.16
10	592	Franklin .....	10.21
5	614	Sarasota .....	19.58
1	621	Okaloosa .....	17.35
3	627	Putnam .....	6.27
1	631	Holmes .....	8.52
4	640-A	Martin .....	9.00
4	640-B	Martin .....	11.80
4	641	Palm Beach .....	13.79
3	642	Putnam .....	10.82
10	644-A	Wakulla .....	7.35
10	645	Franklin .....	6.68
8	647	Highlands .....	9.00

STATE ROAD DEPARTMENT OF FLORIDA  
PLANS COMPLETED 1925-1926—(Continued)

Road No.	Project No.	County	Length
2	648	Hardee .....	10.02
33	649	Okaloosa .....	10.43
10	651	Gulf .....	14.71
33	652	Okaloosa .....	11.50
4	653	Broward .....	13.46
4	654	Broward .....	12.33
1	658	Washington .....	8.20
3	659	Clay .....	13.26
3	660	Clay .....	10.52
5	663	Citrus .....	8.03
4	668	Brevard .....	13.45
27	669	Dade-Collier .....	70.73
6	670	Jackson .....	6.01
1	672	Leon .....	9.92
1	674	Duval .....	3.50
17	675	Polk .....	5.16
19	676-A	Levy .....	9.95
19	676-B	Levy .....	13.14
19	676-C	Levy .....	11.09
13	677-A	Levy .....	6.95
13	677-B	Levy .....	11.58
13	677-C	Levy .....	10.16
13	677-D	Levy .....	7.58
5	679	Hernando .....	7.11
5	682	Citrus .....	6.50
4	683-C	Palm Beach .....	8.27
10	685	Franklin .....	17.23
2	687-A	Lake .....	15.03
2	687-B	Lake .....	15.22
5-A	689	Alachua .....	1.67
4	691	Indian River .....	7.50
4	692	St. Lucie .....	8.93
4	693	St. Lucie .....	12.73
4	694	Martin .....	4.48
2	695	Lake .....	10.50
19	698	Leon .....	12.45
19	699	Jefferson .....	7.70
5-A	718	Columbia .....	8.22
47	727	St. Johns .....	10.50
10	728	Leon .....	11.67
Total Miles.....			1,005.65



Surface Treated Lime Rock Base on Road No. 5, Pasco County.



STATE ROAD DEPARTMENT OF FLORIDA  
TRAFFIC CENSUS REPORT FOR 1925.

Road No.	Location	January		February		March		April		May		June	
		Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks
1	Blackwater River Brg.	521	108	300	51	378	90	460	124	1,483	36	503	149
1	Victory Bridge .....	800	156	.....	.....	221	57	362	95	1,343	33	679	78
1	Ellaville .....	204	13	273	44	232	29	360	105	486	33	509	59
2	White Springs .....	450	17	371	35	411	25	393	38	625	11	568	8
2	High Springs .....	556	67	.....	.....	1,349	176	586	45	604	26	938	42
2	3 Miles South of Ocala	681	83	889	92	738	110	876	93	1,154	36	1,220	102
2	Kissimmee-Orlando ..	1,241	160	1,100	168	630	127	1,114	196	1,769	84	1,490	31
2	Kissimmee-Lakeland ..	560	203	.....	.....	1,224	174	.....	.....	.....	.....	.....	.....
2	Arcadia .....	567	227	646	240	1,053	53	686	193	992	69	796	220
3	Wilds Landing .....	43	8	.....	.....	60	7	83	3	87	5	66	7
3	Orlando-Sanford .....	2,310	221	1,312	218	1,706	251	1,070	173	1,201	195	1,180	102
4	Callahan .....	328	83	385	98	453	113	496	126	1,001	69	602	129
4	North of St. Augustine	583	53	749	97	692	76	693	76	841	157	986	138
4	South of Hastings ..	474	77	642	65	649	116	596	134	537	86	738	79
4	Sharps .....	328	73	683	75	667	63	460	67	.....	.....	346	38
4	Vero .....	.....	.....	1,658	42	1,291	160	1,268	300	.....	.....	1,485	158
4	Dade-Broward .....	1,783	77	1,764	126	1,585	157	.....	.....	.....	.....	.....	.....
5	Montbrook .....	423	32	368	69	352	47	393	88	452	100	463	45
5	Brooksville .....	2,218	824	629	134	529	172	371	136	322	21	358	35
5	Palmetto-Elenton .....	.....	.....	2,212	870	2,924	622	3,903	836	3,230	810	2,176	587
17	Lakeland-Plant City ..	2,051	222	2,618	154	2,318	270	2,618	233	2,664	153	3,007	328
17	Tampa-Oldsmar .....	1,821	179	1,629	197	1,486	188	1,802	200	1,812	200	1,758	225
24	Kissimmee .....	445	14	310	41	425	23	423	18	352	22	671	7

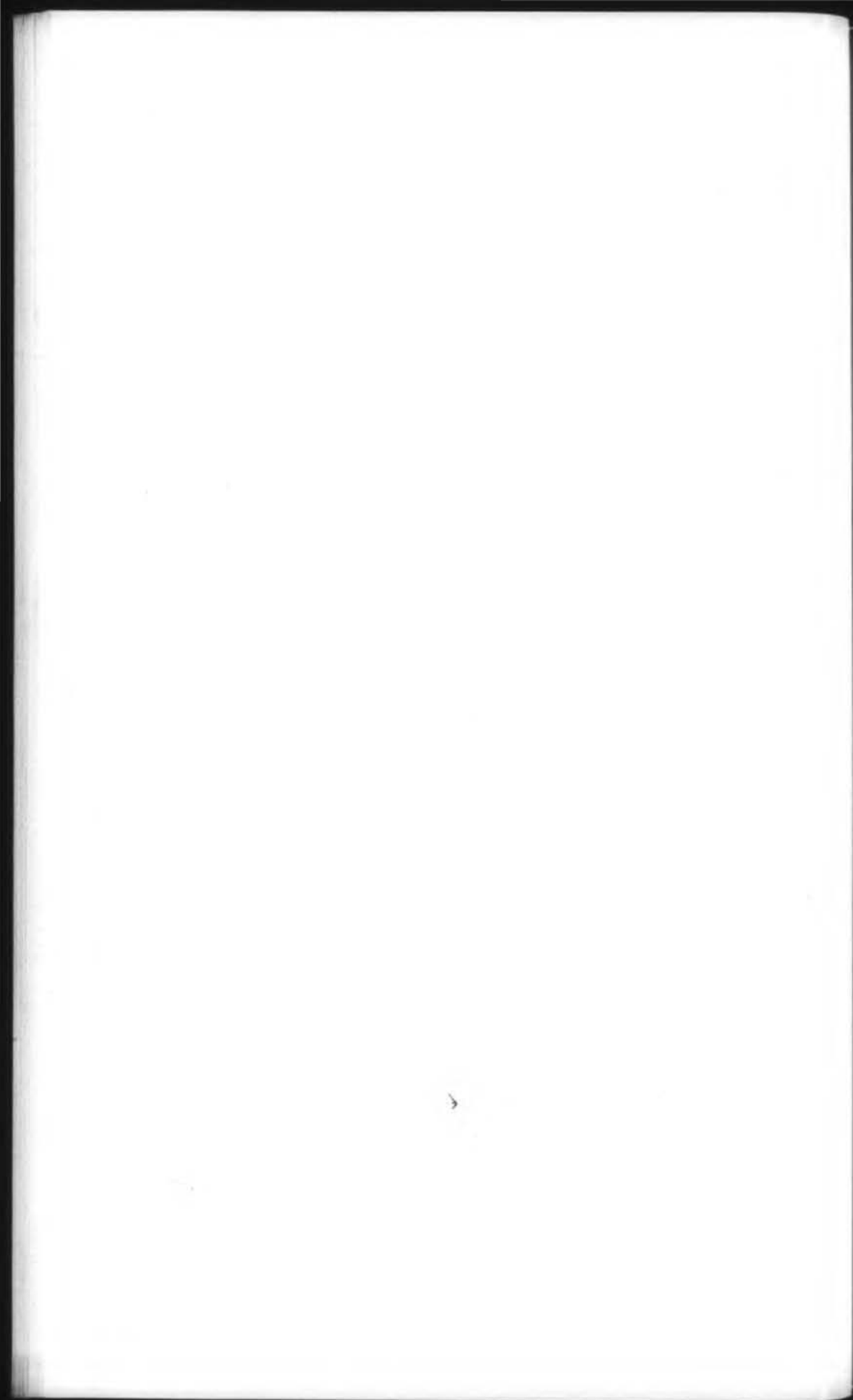
STATE ROAD DEPARTMENT OF FLORIDA  
TRAFFIC CENSUS REPORT FOR 1925— (Continued)

Road No.	Location	July		August		September		October		November		December	
		Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks
1	Blackwater River Brg.	444	157	610	142	326	105	517	96	534	98	633	112
1	Victory Bridge .....	565	87	528	76	512	82	488	67	416	97	605	48
1	Ellaville .....	660	50	709	57	628	90	627	73	693	107	1,246	101
2	White Springs .....	744	37	845	38	.....	.....	1,008	112	842	98	1,223	178
2	High Springs .....	1,037	56	1,346	52	1,759	94	1,829	145	1,349	169	2,064	161
2	3 Miles South of Ocala	1,376	91	1,421	143	1,495	148	1,180	34	1,209	264	2,069	201
2	Kissimmee-Orlando ..	1,674	50	2,163	26	2,215	33	1,502	66	2,037	39	3,383	58
2	Kissimmee-Lakeland ..	.....	.....	3,229	213	.....	.....	2,883	28	3,290	317	2,721	270
2	Arcadia .....	1,029	69	893	88	594	134	817	149	750	157	.....	.....
3	Wilds Landing .....	74	2	98	4	117	14	168	.....	147	15	217	2
3	Orlando-Sanford .....	.....	.....	1,014	265	1,195	109	1,144	176	1,389	215	1,915	346
4	Callahan .....	940	128	892	91	1,140	120	1,161	152	748	184	834	128
4	North of St. Augustine	1,095	118	1,359	156	1,416	252	1,529	307	1,443	377	2,090	517
4	South of Hastings ...	893	96	1,080	126	1,144	102	1,289	94	1,389	220	1,553	212
4	Sharpes .....	320	53	492	55	1,258	125	2,152	184	2,136	287	1,640	.....
4	Vero .....	2,031	168	2,430	134	2,196	246	2,069	21	2,662	381	3,143	466
4	Dade-Broward .....	3,453	834	4,402	1,023	4,883	1,204	6,674	1,059	6,259	1,530	6,308	1,065
5	Montbrook .....	395	50	428	40	419	60	905	61	578	59	801	76
5	Brooksville .....	307	2	413	34	606	59	717	58	545	82	1,009	102
5	Palmetto-Elleston ...	1,627	228	2,654	939	4,240	920	4,559	1,390	3,617	936	4,051	1,170
17	Lakeland-Plant City ..	2,552	22	2,350	534	3,330	346	3,404	385	2,985	296	5,072	419
17	Tampa-Oldsmar .....	1,791	198	1,817	200	2,064	216	2,325	208	1,957	285	2,735	251
24	Kissimmee-Melbourne ..	387	3	441	7	555	14	271	24	504	15	432	59





F. A. Project 38—Escambia Bay Bridge Showing Fill and Guard Rail.



STATE ROAD DEPARTMENT OF FLORIDA  
TRAFFIC CENSUS REPORT FOR 1926.

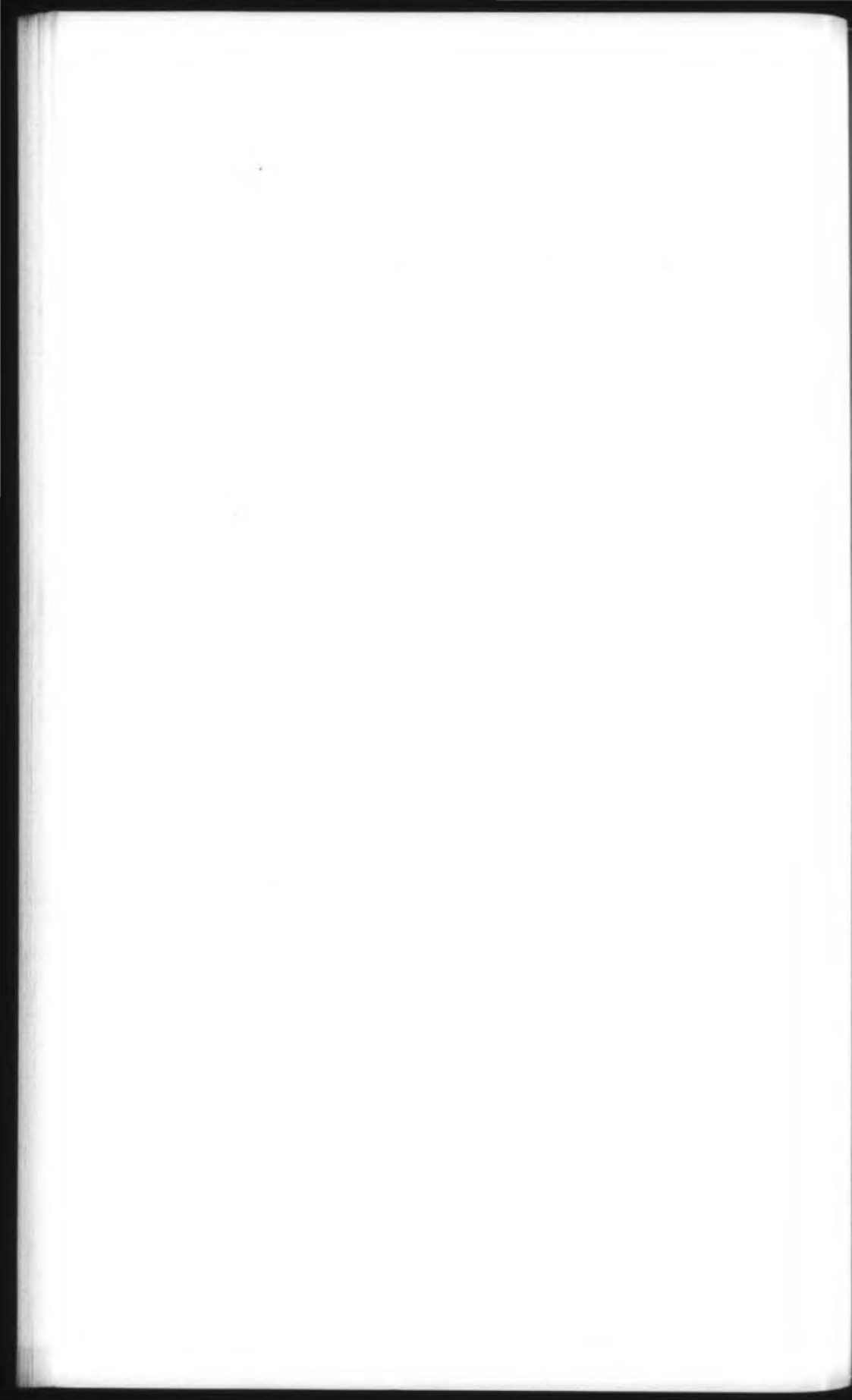
Road No.	Location	January		February		March		April		May		June	
		Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks
1	Blackwater River Brg.	408	28	447	86	539	116	.....	.....	593	165	.....	.....
1	Victory Bridge .....	570	28	331	96	559	90	610	83	533	91	843	67
1	Ellaville .....	656	52	563	71	657	176	634	94	681	59	817	104
2	White Springs .....	1,152	105	1,199	152	904	107	1,552	106	.....	.....	974	68
2	High Springs .....	594	46	739	74	1,711	141	1,282	114	1,185	70	1,778	100
2	3 Miles South of Ocala	883	80	.....	.....	1,305	167	.....	.....	1,415	182	1,051	78
2	Kissimmee-Orlando ..	.....	.....	3,146	158	2,526	251	2,300	385	2,725	176	2,539	94
2	Kissimmee-Lakeland ..	2,654	250	1,874	214	3,600	.....	2,995	304	.....	.....	2,579	122
2	Arcadia .....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....
3	Wilds Landing .....	3,407	100	115	14	119	21	201	7	152	3	120	9
3	Orlando-Sanford .....	1,969	506	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....
4	Callahan .....	1,080	47	580	142	970	159	989	137	910	109	579	106
4	North of St. Augustine	979	240	2,181	379	1,587	343	1,429	213	975	166	913	180
4	South of Hastings ...	1,343	201	987	181	930	103	928	151	447	49	634	42
4	Sharps .....	1,746	.....	1,937	.....	2,095	.....	1,515	.....	1,520	141	1,421	.....
4	Vero .....	2,464	462	2,818	557	2,635	657	3,021	427	2,339	609	1,716	408
4	Dade-Broward .....	6,285	1,258	7,170	1,705	6,343	1,522	6,331	984	4,762	827	4,458	1,005
5	Montbrook .....	601	40	527	66	576	40	655	65	747	139	743	85
5	Brooksville .....	814	51	571	124	676	.....	907	99	428	63	565	83
5	Palmetto-Ellenton ..	2,424	639	1,984	167	1,959	51	.....	.....	.....	.....	6,269	1,933
17	Lakeland-Plant City ..	.....	.....	.....	.....	.....	.....	.....	.....	2,493	426	1,927	386
17	Tampa-Oldmar .....	2,676	282	3,281	292	3,006	285	2,455	260	1,775	257	1,911	158
24	Kissimmee-Melbourne ..	1,152	87	724	50	557	28	1,167	30	470	96	469	e

STATE ROAD DEPARTMENT OF FLORIDA  
TRAFFIC CENSUS REPORT FOR 1926—(Continued)

Road No.	Location	July		August		September		October		November		December	
		Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks
1	Blackwater River Brg.	940	213	834	170	.....	.....	854	24	686	165	1,255	302
1	Victory Bridge .....	793	95	615	127	671	43	843	23	694	102	1,212	50
1	Ellaville .....	844	62	724	51	536	68	454	19	429	49	787	44
2	White Springs .....	1,117	56	840	84	1,058	110	1,231	107	1,095	150	1,237	114
2	High Springs .....	1,844	113	1,624	88	1,214	124	1,644	145	3,141	265	5,268	190
2	3 Miles South of Ocala	1,480	66	1,190	91	1,101	111	.....	.....	.....	.....	.....	.....
2	Kissimmee-Orlando .....	.....	.....	.....	.....	.....	.....	2,101	370	2,186	.....	3,727	122
2	Kissimmee-Lakeland .....	2,778	275	3,566	.....	.....	.....	1,991	.....	.....	.....	1,735	147
2	Arcadia .....	.....	.....	.....	.....	.....	.....	.....	.....	661	112	.....	.....
3	Wilds Landing .....	134	7	130	4	185	7	.....	.....	.....	.....	.....	.....
3	Orlando-Sanford .....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....
4	Callahan .....	.....	.....	926	108	795	128	745	27	825	190	1,066	148
4	North of St. Augustine	1,004	128	941	101	921	194	1,160	131	1,325	194	1,390	207
4	South of Hastings .....	697	117	606	128	763	76	624	60	864	72	1,167	77
4	Sharps .....	1,019	.....	970	.....	1,614	.....	1,305	.....	1,319	.....	1,398	.....
4	Vero .....	2,280	266	2,207	255	1,920	286	2,313	244	2,172	253	2,663	282
4	Dade-Broward .....	3,633	679	2,909	924	2,743	867	3,633	842	4,404	1,433	4,594	973
5	Monthbrook .....	710	59	514	67	419	60	648	64	649	60	1,135	67
5	Brooksville .....	447	90	365	61	277	48	681	57	644	82	585	128
5	Palmetto-Ellenton .....	.....	.....	.....	.....	.....	.....	.....	.....	3,504	294	2,826	450
17	Lakeland-Plant City .....	1,947	448	2,045	344	1,912	356	1,890	342	1,960	390	2,351	314
17	Tampa-Oldmar .....	1,892	150	1,580	204	1,498	231	1,905	194	1,650	217	2,003	194
24	Kissimmee-Melbourne .....	1,119	50	498	.....	345	.....	490	.....	348	.....	400	.....



State Road No. 8 Across Indian Prairie.



## CONVICT LABOR

The State Convict Road Force is the oldest institution connected with the State Road Department, and is the nucleus around which the present working force has been built.

Since 1917 State convicts have been used by this Department in the construction of roads. The number of convicts first granted to the Department was small, but this has been increased from time to time until at present there are working on the State system of roads under the supervision of this Department approximately one thousand convicts.

For one or two years this Department used convicts to do all classes of road work, but during the past several years it has been the rule of the Department to confine the work of the convict force to clearing, grubbing and grading of highways.

The convict force is one of the greatest assets the State has for road building purposes, as pointed out in the accompanying tables. During the past two years the convicts worked 421,913.3 days on the roads, which represents a money value of approximately \$1,265,739.90, based on the present day wages paid to free labor.

The State Road Department bears the cost of housing, feeding, clothing and guarding the prisoners making up the road force, which amount, according to the tabulations shown herein, is \$1.064 per calendar day, or \$1.452 per working day.

One of the chief advantages of working prisoners on the roads is that it gives them good, wholesome exercise, which promotes better health than if confined in the penitentiary.

At the present time the Department has sixteen camps of convicts, the average number to each camp being 61.7. The camps are located in the following counties: Jefferson, Brevard, Okaloosa, Charlotte, Leon, Gulf, Collier, Mad-

ison, Jackson, Gadsden, Wakulla; working on roads Nos. 1, 6, 10, 18, 19, 22 and 35.

The efficiency of the convicts under the present system, although showing a cost lower than free labor, is not up to what it should be, especially among the white camps.

The quality of work performed by the prisoners is of the highest and stands out in appearance far ahead of contract work of similar nature.

It has been the policy of this Department during this biennial to concentrate the prisoners as much as possible in one section, as by this method a closer supervision can be maintained and a certain amount of rivalry among the camps adds to the efficiency.

In addition to the centralized working of prisoners the Department recognized the importance of a closer supervision, especially with reference to their care and also transfer from farm to camps and general supervision as to complaints, and, therefore, appointed a Supervisor of Convicts, this appointment being made in September of 1926. Since that time the Supervisor has rendered valuable service and shown a saving especially in the transfer of prisoners and the return of escaped ones.

The Department in connection with its convict work has also added to the organization a Veterinarian to attend to the mules owned by the Department, which number at the present time is 514. His duties being to supervise the general care and advise the men in the proper care and feeding, also to render such medical attention as may be needed to keep all stock in good working condition. The care and workable condition of the stock is just as important as the care and condition of the convicts. The stock represents a considerable investment.

Summarizing the work done by prisoners: There have been 254 miles of right of way cleared; 259 miles of road graded; 131 miles of road clay surfaced; 44 miles of rock base laid; also convict labor was used in the construction of 290 culverts.



STATE ROAD DEPARTMENT OF FLORIDA  
STATE CONVICT CAMPS—1925-1926

Camp No.	CAPTAIN	Average No. of Prisoners	Total Time Days	Total Time Road	Time on Yard Holidays, etc.	Cost of Camp
1	J. W. Daniels .....	62.25	40,961.5	29,222.8	11,738.7	\$ 39,570.49
2	A. L. Conyers .....	77.92	47,474.6	34,216.5	13,258.1	51,355.70
3	I. E. Steel .....	59.17	39,630.7	28,638.7	10,992.0	44,783.26
4	H. M. Thomas .....	36.08	25,187.0	18,142.0	7,045.0	30,883.41
5	Tyson Foster .....	51.92	35,673.0	26,754.5	8,918.5	40,947.49
8	Evans Waldrop .....	59.79	41,572.0	31,463.0	10,109.0	43,807.96
12	C. W. Hansford .....	45.92	31,361.5	21,783.1	9,578.4	37,164.65
*15		56.10	16,049.0	12,155.5	3,893.5	16,063.89
16	A. C. McGlew .....	49.17	34,376.1	25,276.8	9,099.3	36,685.89
18	J. C. Hansford .....	44.04	29,581.0	21,675.0	7,906.0	28,270.17
*19		29.41	17,765.0	13,363.5	4,401.5	22,013.91
*22		44.33	11,325.0	7,941.4	3,383.6	13,612.97
26	M. Willis .....	44.62	31,942.0	24,562.5	7,379.5	30,437.97
27	A. Coursey .....	53.50	36,617.0	26,086.9	10,530.1	40,768.45
28	L. T. Taylor .....	45.42	31,870.0	22,685.7	9,184.3	34,163.61
*29		30.00	4,113.0	2,837.2	1,275.8	4,409.27
30	M. M. Robinson .....	53.04	35,857.0	27,784.9	8,072.1	33,880.21
31	Alex Patterson .....	58.88	40,740.0	30,502.6	10,237.4	37,085.58
32	J. R. Land .....	26.50	8,954.4	6,394.1	2,560.3	12,102.34
33	J. E. Totum .....	59.89	14,762.5	10,426.6	4,335.9	14,206.37
34	J. E. Mashburn .....	.....	.....	.....	.....	556.72
	Total .....	987.95	575,812.3	421,913.3	153,899.0	\$ 612,770.31

NOTE: Camps marked \* have been discontinued and prisoners assigned to other camps.

## TESTING DIVISION

The laboratory of the Department is located about one mile northeast of Gainesville on Road No. 14, where it has been maintained for the past two years. Previous to that time quarters were furnished by the College of Engineering of the University of Florida.

During 1925 and 1926 over 6,000 samples were tested. Rock for base and sand for concrete construction composed the majority of the samples, but many samples of coarse aggregates, cement, bituminous materials and mixtures, concrete, paint and reinforcing steel were tested.

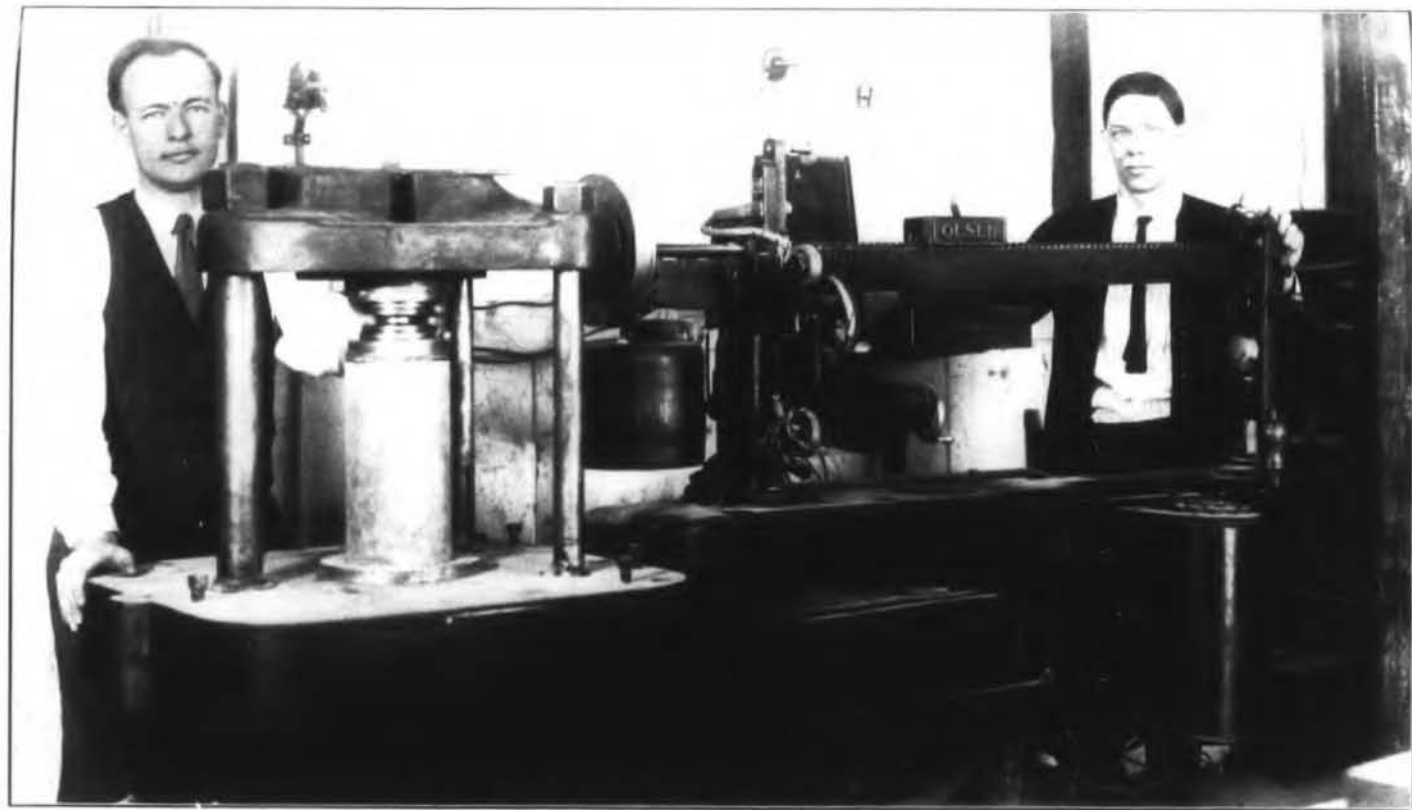
The Testing Division carries a stock of sample bags, information cards, tags and moulds for concrete cylinders which are furnished to the field men upon request. On projects where plant inspection is necessary report forms are supplied together with some of the equipment required, the remaining equipment being transferred from other projects or purchased direct when not available.

Samples are received daily from the Post Office and Express Company. Each sample is recorded by number and the necessary tests made. In general the work is divided into a chemical laboratory and a concrete laboratory, each occupying 1,500 square feet of floor space.

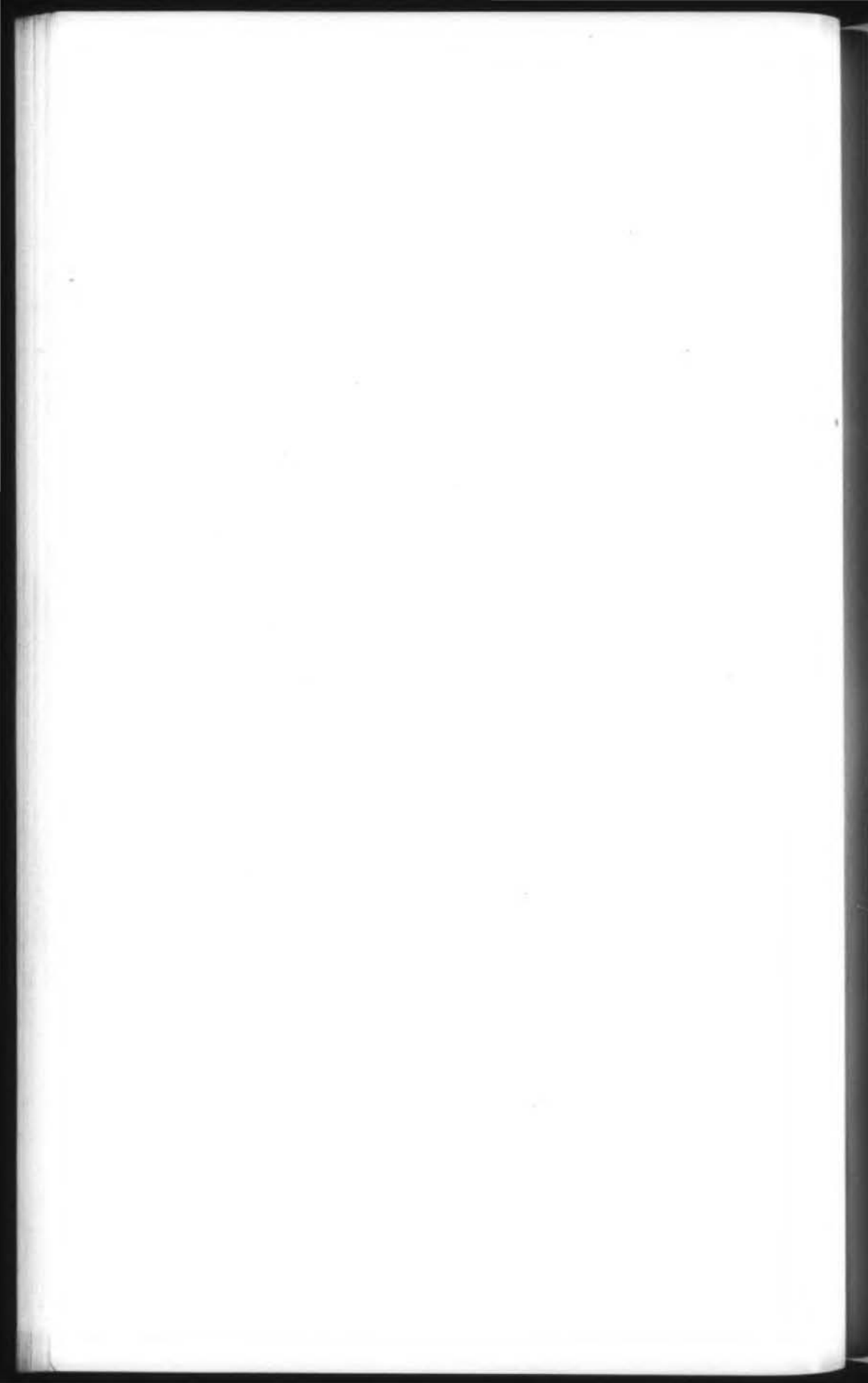
The chemical laboratory is equipped with a large hood, work tables and apparatus for testing asphalt, tar, metals, paint and rock for base. In this laboratory is the sieving apparatus for fine aggregates and extractors for separating bitumen from mixed pavements.

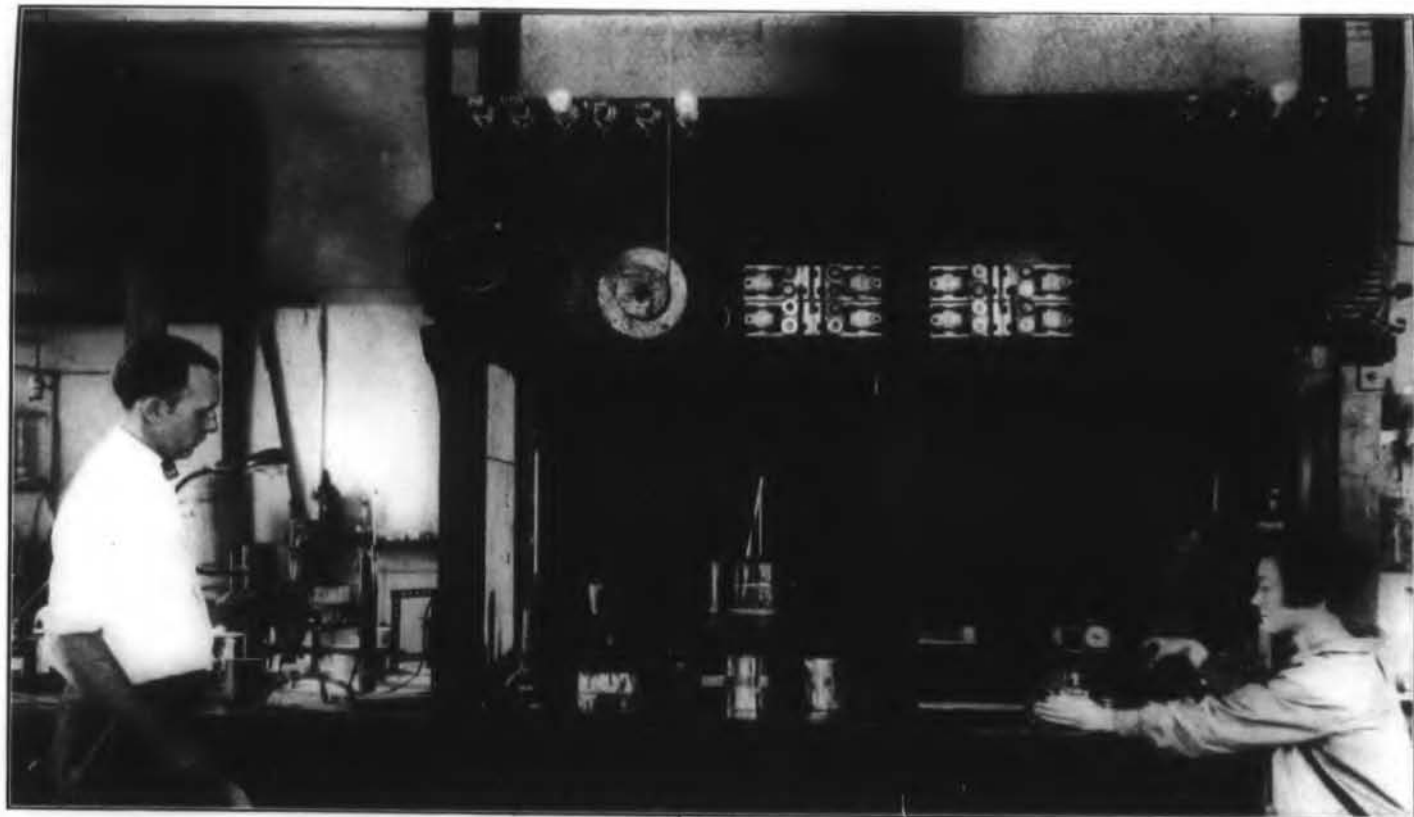
The concrete laboratory contains most of the equipment for cement testing, and the heavy apparatus required for testing road materials. There are testing machines of 20,000 and 100,000 pounds capacity, Deval abrasion machine, a rock crusher and a small grinder.

Coarse aggregates are separated on laboratory screens into sizes as required. Lime rock samples are screened,



Laboratory Equipment used in Testing Road Materials.





View of Section Chemical Laboratory.

crushed and a portion dried and ground for chemical analysis. Fine aggregates, cement and water for concrete are made into standard mortar specimens and tested in comparison with Ottawa sand. Aggregates may be made into concrete specimens and the compressive strength determined. Specimens of concrete from the field are stored until the proper age is reached and tested for crushing strength.

All results are recorded on 5x8-inch cards for the permanent files. Copies of the results are reported to the office at Tallahassee, to the Division Engineer concerned, to the field engineer or person submitting the sample, and in case of Federal Aid projects two copies are delivered to the representative of the Bureau of Public Roads.

On December 31st, 1926, the personnel of the laboratory consisted of H. A. Hall, Testing Engineer; R. L. Jenkins and Miss N. J. Morris, Chemists, and R. M. Smith, W. H. Fisher and L. S. Marshall, Laboratory Assistants.

## FEDERAL AID

The Federal Government aids the several States in the construction of certain trunk line highways in accordance with certain acts passed by the Congress of the United States and under the rules and regulations adopted by the Bureau of Public Roads and approved by the Secretary of Agriculture. The roads on which Federal Aid may be applied comprise what is designated as the Federal Seven Percent System, that is this system may comprise not more than seven percent of the total public road mileage of the State. According to the present regulations no Federal funds may be applied to any roads other than those on this seven per cent system until this system has been completed. This does not mean that all the roads on this seven percent system shall necessarily be built with the use of Federal aid, but may be built with State or County funds, but must be built of a type and in a manner acceptable to the Bureau of Public Roads. The participation of the Federal Government in the cost of any road is fifteen thousand dollars per mile, not including bridges having a clear span of more than twenty feet. The Government will participate in the cost of bridges having a clear span of more than twenty feet to fifty percent of the cost of such bridge. There seems to be an erroneous idea as to the amount of Federal aid a State may receive during a certain year. Regardless of the amount of money which may be made available by the State authorities, only a certain fixed amount may be received from the Government. Florida has been receiving something less than nine hundred thousand dollars per year for the last four years and may expect, under existing Federal legislation, to receive a similar amount for two years to come. The Federal aid is apportioned to the several States in the following manner: One-third in the ratio which the area of each State bears to the total area of all the States; one-third in the ratio

which the population of each State bears to the total population of all the States as shown by the latest available Federal census, and one-third in the ratio which the mileage of rural delivery routes and star routes in each State bears to the total mileage of rural delivery and star routes in all the States at the close of the next preceding fiscal year as shown by certificate of the Postmaster General, which he is directed to make and furnish annually to the Secretary of Agriculture.

It will be noted that during the past four years, although operating under laws which made available the same amount annually as Federal Aid, Florida's proportion has not been exactly the same during any two years. This is due to the increased mileage of rural delivery and star routes as well as to certain refunds in the way of contingencies which were not used up and were turned back to the general fund and reapportioned to the several States.

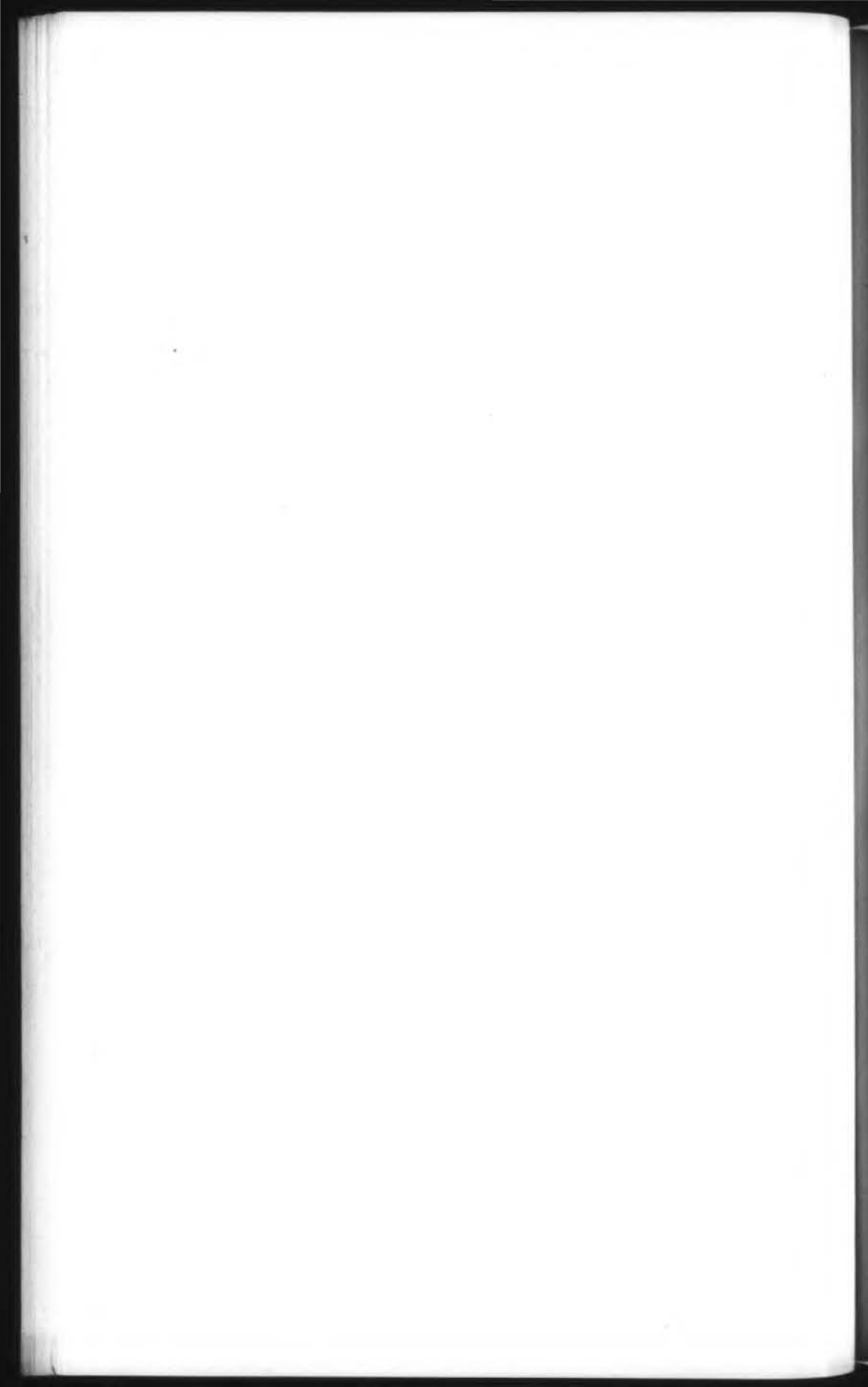
The Federal Government does not participate in the cost of the surveys necessary in the location of the project, the making of the plans and specifications nor in the maintenance of the road after it has been built. The Government does, however, exercise certain jurisdiction over Federal Aid projects after completion and according to law require that they be maintained in an acceptable manner.

Certain tables found in this report will show the amount of Federal Aid appropriated annually since the enactment of the Federal Aid Road Act as well as the amount which has been received by the State of Florida.





View Showing use of Water Proofing of Concrete over or near Salt Water.



STATE ROAD DEPARTMENT OF FLORIDA  
UNITED STATES BUREAU OF PUBLIC ROADS  
SUMMARY OF APPROPRIATION TO ALL THE STATES

Year	Law of 1916	Amendment 1919	Amendment 1921	Amendment 1922	Entire United States
1917	\$ 5,000,000.00	\$ .....	\$ .....	\$ .....	\$ 5,000,000.00
1918	10,000,000.00	.....	.....	.....	10,000,000.00
1919	15,000,000.00	50,000,000.00	.....	.....	65,000,000.00
1920	20,000,000.00	75,000,000.00	.....	.....	95,000,000.00
1921	25,000,000.00	75,000,000.00	25,000,000.00	.....	125,000,000.00
1922	.....	.....	50,000,000.00	.....	50,000,000.00
1923	.....	.....	.....	50,000,000.00	50,000,000.00
1924	.....	.....	.....	65,000,000.00	65,000,000.00
1925	.....	.....	.....	75,000,000.00	75,000,000.00
1926	.....	.....	.....	75,000,000.00	75,000,000.00
1927	.....	.....	.....	75,000,000.00	75,000,000.00
1928	.....	.....	.....	75,000,000.00	75,000,000.00
Total	\$ 75,000,000.00	\$200,000,000.00	\$ 75,000,000.00	\$415,000,000.00	\$ 765,000,000.00

PRO RATA ALLOTTED TO FLORIDA

Year	Law of 1916	Amendment 1919	Amendment 1921	Amendment 1922	Total to Florida
1917	\$ 55,976.27	\$ .....	\$ .....	\$ .....	\$ 55,976.27
1918	111,952.54	.....	.....	.....	111,952.54
1919	170,723.88	573,797.20	.....	.....	744,521.08
1920	229,518.88	860,695.79	.....	.....	1,090,214.67
1921	286,861.98	860,585.94	.....	.....	1,147,447.92
1922	.....	.....	886,825.69	.....	886,825.69
1923	.....	.....	.....	*599,221.13	599,221.13
1924	.....	.....	.....	771,395.18	771,395.18
1925	.....	.....	.....	887,336.52	887,336.52
1926	.....	.....	.....	892,878.00	892,878.00
1927	.....	.....	.....	897,185.00	897,185.00
1928	.....	.....	.....	899,451.00	899,451.00
Total	\$ 855,033.55	\$ 2,295,078.93	886,825.69	4,947,466.83	8,984,405.00

\*Includes \$8,004.00 Reapportionment from Montana.

STATE ROAD DEPARTMENT OF FLORIDA  
RECORD OF FEDERAL AID PROJECTS TO DECEMBER 31, 1926

Road No.	Proj. No.	County	Length Miles	Type	Total Cost Estimated	Amount Requested	Amount Granted	Amount Secured
1	1	Okaloosa	10.45	Sand Clay	\$ 34,083.28	\$ 17,041.64	\$ 17,041.64	\$ 16,938.10
2	2	Osceola	4.12	Brick	69,678.53	20,000.00	20,000.00	20,000.00
1	3	Gadsden-Jackson	0.19	Concrete Arch	417,031.12	208,515.56	208,515.56	208,515.56
2	4	DeSoto	3.03	Concrete	66,005.98	30,000.00	30,000.00	30,000.00
18	5	DeSoto	2.50	Concrete	46,879.17	20,000.00	20,000.00	20,000.00
1	6	Madison	5.60	Sand Clay	369,932.62	124,814.25	124,814.25	12,914.61
2	7A-B	Hamilton	13.047	R. B., Concrete Arch.	467,299.91	227,826.56	227,826.56	104,395.29
2	8	Alachua	11.73	Bituminous Macadam	217,155.38	108,577.69	108,577.69	108,577.69
1	9	Holmes	5.11	Sand Clay	25,525.06	12,762.53	12,762.53	12,762.53
1	11	Baker-Nassau-Duval	15.40	Concrete	560,421.41	281,710.70	281,710.70	248,115.83
2	13	Orange	8.59	Sheet Asphalt	305,686.15	152,843.07	152,843.07	134,518.71
1	14A-B	Santa Rosa	6.40	Brick, Concrete & Steel	284,174.26	142,087.13	142,087.13	139,691.18
2	15	DeSoto	3.26	Concrete	54,570.67	20,000.00	20,000.00	20,000.00
18	16	DeSoto	0.67	Concrete	11,798.80	5,899.40	5,899.40	5,245.57
5	17	Manatee	7.87	Bituminous Macadam	264,953.11	132,476.55	132,476.55	103,867.12
3	18	Putnam	12.79	Bituminous Macadam	454,730.74	227,365.37	227,365.37	171,022.41
2	19-A	Hamilton	8.162	Rock Base				
2	19-B	Hamilton	0.032	Concrete	239,766.77	114,414.27	114,414.27	101,377.01
1	20	Holmes & Washington	0.38	Concrete	370,976.46	185,488.23	185,488.23	154,128.95
1	21-A	Baker	5.70	Concrete				
1	21-B	Baker	0.064	Concrete	234,656.07	117,328.03	117,328.03	102,240.83
1	22	Baker	8.90	Concrete	284,984.50	142,492.25	142,492.25	126,671.93
1	23	Duval	12.80	Concrete	467,017.29	233,508.64	233,508.64	220,570.86
1	24	Columbia	9.89	Bituminous Macadam	348,171.92	174,085.96	174,085.96	143,595.12
2	26	Columbia	11.01	Concrete	370,697.14	178,358.52	178,358.52	155,996.28
2	27A-B	Columbia	12.36	Concrete	541,991.98	252,245.22	252,245.22	252,245.22
5	29A-B	Hillsborough	0.06	Concrete	148,680.79	74,340.39	74,340.39	74,340.39
2	30	Columbia-Hamilton	0.061	Concrete & Steel	63,033.24	26,991.69	26,991.69	26,991.69
2	31	Hamilton	11.81	Rock Base	294,287.60	144,982.64	144,982.64	127,764.66
4	32	Nassau	10.00	Bituminous Macadam	369,717.21	184,858.60	184,858.60	179,131.29
2	33	Alachua	12.60	Bituminous Macadam	358,844.39	179,422.19	179,422.19	171,863.17
7	34	Escambia	10.00	Concrete	297,291.77	148,645.88	144,645.88	139,795.19

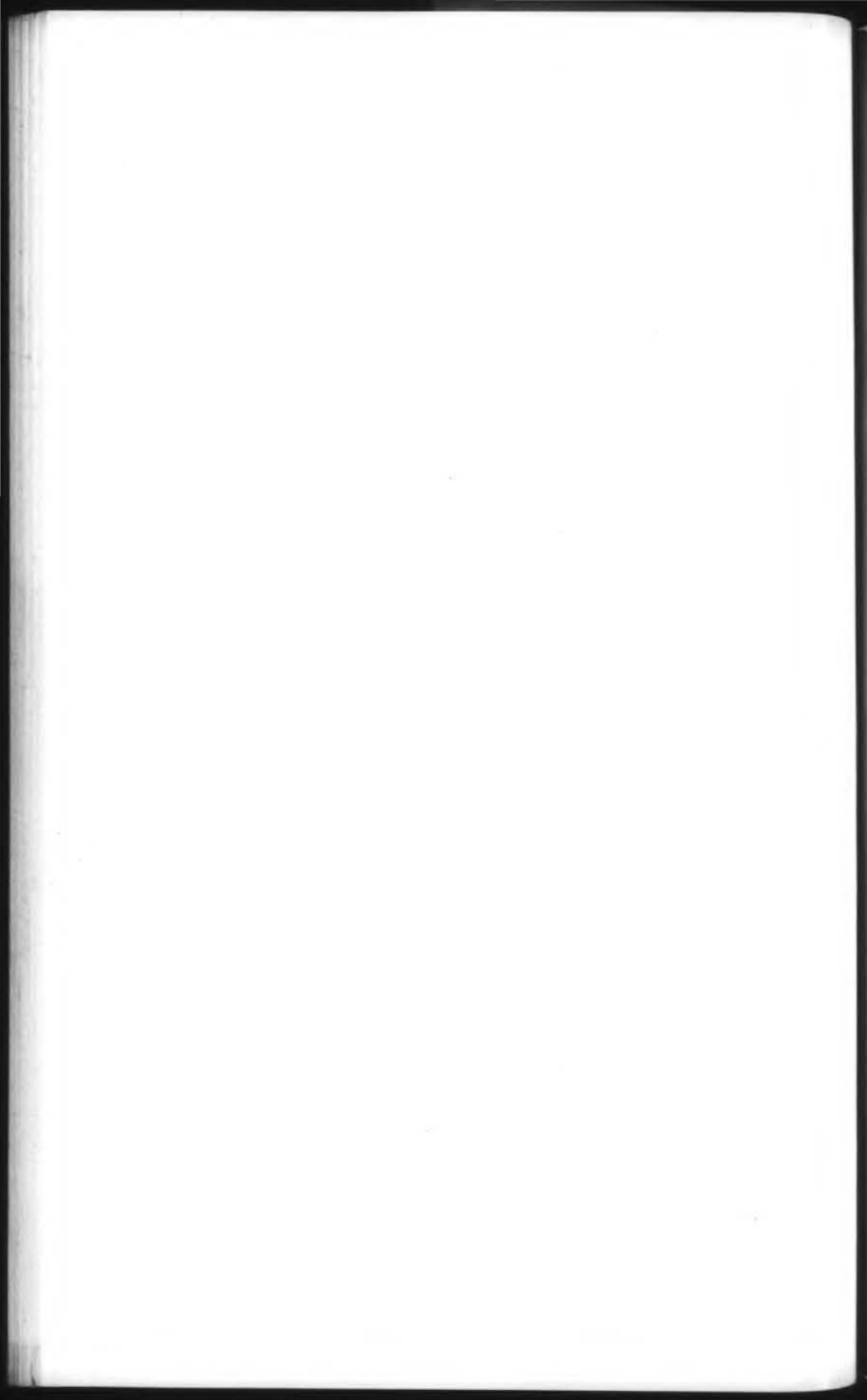
1	35	Escambia	5.00	Concrete	152,411.86	76,205.93	76,205.93	72,890.22
4	36-A	St. Lucie	7.76	Bituminous Macadam	277,105.50	138,462.20	138,462.20	118,623.95
4	36-B	St. Lucie	7.12	Concrete	288,228.60	142,420.00	142,420.00	113,887.26
2	37A-C	Alachua	3.96	Sheet Asphalt	180,779.29	90,300.93	90,300.93	87,880.36
2	37-B	Alachua	.191	Concrete	39,256.28	19,628.14	19,628.14	.....
2	37-D	Alachua	2.14	Graded	172,898.02	48,347.07	48,347.07	36,551.42
2	37-E	Alachua	7.96	Sheet Asphalt	332,870.41	159,240.00	159,240.00	159,240.00
1	38-A	Escambia	3.20	Timber Bridges, Approach Embankment	853,686.57	426,843.28	426,843.28	316,973.89
1	38-B	Escambia	0.29	Steel Bridge				
4	39A-B	Brevard & St. Lucie	0.426	Concrete & Dredge Embankment	232,913.55	116,456.77	116,456.77	98,913.59
4	40-A	Brevard	16.169	Rock base	446,546.93	223,273.46	223,273.46	202,126.88
4	40-B	Brevard	0.025	Concrete	62,178.21	31,089.10	31,089.10	.....
4	40-D	Brevard	6.727	Rock Base	158,783.06	79,391.53	79,391.53	69,353.09
4	40-E	Brevard	13.596	Rock Base	315,218.29	157,609.14	157,609.14	142,613.69
4	41	Dade	12.00	Sheet Asphalt	692,319.40	214,906.05	214,906.05	52,039.11
2	43	Marion	10.534	Sheet Asphalt	381,991.44	173,581.29	173,581.29	148,154.25
2	44	Lake	10.529	Bituminous Concrete	395,611.38	165,565.84	165,565.84	145,995.59
1	45	Suwannee & Madison	0.173	Concrete and Steel	124,902.27	62,451.13	62,451.13	51,318.41
3	46	Nassau	11.526	Concrete	627,146.75	204,358.70	204,358.70	68,216.75
4	47	St. Johns	14.88	Concrete	573,849.99	241,341.37	241,341.37	72,443.93
4	48A-B	St. Johns	15.39	Rock Base & Concrete Bridge	602,501.02	279,935.45	279,935.45	64,812.12
4	49A-B	Flagler	13.81	Graded	183,633.91	91,816.95	91,816.95	18,007.06
14	50-A	Putnam	6.19	Bituminous Macadam	412,556.78	133,896.78	133,896.78	.....
14	50-B	Putnam	9.77	Graded	152,128.02	76,064.01	76,064.01	40,225.43
14	50-C	Putnam	10.03	Bituminous Macadam	519,013.59	150,450.00	150,450.00	.....
7	51	Escambia	6.28	Concrete	206,575.47	94,302.00	94,302.00	79,953.52
1	52	Escambia	10.09	Graded	241,904.49	120,952.24	120,952.24	.....
2	53-A	Lake	7.108	Bituminous Concrete	309,286.93	106,620.00	106,620.00	8,563.14
2	53-C	Lake	1.876	Graded	113,087.04	28,050.00	28,050.00	.....
1	54	Leon	19.236	Rock Base & Concrete	501,780.21	250,890.10	250,890.10	.....
1	58	Jefferson	6.309	Rock Base	117,466.26	58,733.13	58,733.13	.....
1	59	Jefferson	9.107	Concrete	266,053.37	133,026.68	133,026.68	.....
3	421	Nassau	0.106	Concrete & Steel	258,417.08	129,208.54	129,208.54	51,417.13
Totals			474.094		\$18,236,144.61	\$ 8,314,500.77	\$ 8,314,500.77	\$5,453,475.98

STATE ROAD DEPARTMENT OF FLORIDA  
PLANS FOR BRIDGES 20 FEET AND OVER IN LENGTH  
Prepared by Bridge Department—1925-1926

Road No.	Proj. No.	COUNTY	No. of Bridges	Type			Total Length
				Concrete	Steel	Timber	
1621		Okaloosa	3	.....	.....	2,820	2,820
1621-B		Okaloosa	1	1,344	120	.....	1,464
1621-C		Okaloosa	1	1,240	120	.....	1,360
1621-D		Okaloosa	1	30	.....	.....	30
1631-B		Holmes	3	.....	.....	1,480	1,480
1633-B		Gadsden	1	120	.....	.....	120
1634-B		Jackson	1	336	120	.....	456
1638		Santa Rosa	1	.....	.....	390	390
1658-B		Washington	4	.....	.....	510	510
1672		Leon	1	.....	.....	140	140
1672-B		Leon	2	1,827	120	.....	1,947
1673		Gadsden	1	20	.....	.....	20
1673-B		Gadsden	1	336	80	.....	416
1697		Escambia	1	.....	.....	488	488
2648-B		Hardee	6	140	80	375	595
3421		Florida-Georgia	1	200	360	.....	560
3543		Seminole	1	43	.....	.....	43
3642		Putnam	5	.....	.....	195	195
3659		Clay	3	.....	.....	285	285
3660		Clay	2	.....	.....	90	90
3664		Clay	1	1,426	160	.....	1,586
3665		Clay	1	1,209	160	.....	1,369
440-B		Brevard	1	112	.....	.....	112
441		Dade	3	180	.....	.....	180
448-B		St. Johns	3	456	.....	.....	456
449-B		Flagler	4	207	.....	.....	207
4641-B		Palm Beach	2	150	97	.....	247
4656		Palm Beach	1	630	97	.....	727
4691		Indian River	1	110	.....	.....	110
4692		St. Lucie	1	86	.....	.....	86
4693		St. Lucie	5	160	.....	.....	160
4694		Martin	6	160	.....	.....	160
5564-C		Charlotte	2	63	80	.....	143
5581		Hillsborough	5	203	.....	.....	203
5614		Sarasota	4	148	.....	.....	148
10637		Leon	1	30	.....	.....	30
10645		Wakulla	2	.....	.....	605	605
10645-B		Wakulla	1	.....	.....	850	850
10685		Franklin	4	.....	.....	150	150
13677-B		Levy	2	.....	.....	285	285
13677-C		Levy	12	.....	.....	855	855
1450-B		Putnam	2	143	.....	.....	143



Concrete Bridge near Chattahoochee.





STATE ROAD DEPARTMENT OF FLORIDA  
PLANS FOR BRIDGES 20 FEET AND OVER IN LENGTH  
(Continued)

Prepared by Bridge Department—1925-1926

Road No.	Proj. No.	COUNTY	No. of Bridges	Type			Total Length
				Concrete	Steel	Timber	
17 675		Polk .....	1	54	.....	.....	54
18 655		Highlands .....	8	.....	.....	345	345
18 667		DeSoto .....	6	.....	.....	255	255
19 580		Dixie .....	21	.....	.....	1680	1680
19 676-B		Levy .....	2	.....	.....	150	150
20 500-A		Bay .....	2	.....	.....	130	130
20 500-B		Bay .....	2	.....	.....	900	900
20 501		Calhoun .....	2	.....	.....	210	210
27 669-B		Dade .....	3	.....	.....	260	260
27 669-C		Dade .....	22	.....	.....	1,065	1,065
27 669-X		Dade .....	10	.....	.....	300	300

RAILROAD OVERHEAD CROSSINGS

1 621	Okaloosa .....	1	.....	.....	165	165
1 633-B	Gadsden .....	1	120	.....	.....	120
1 657	Jackson .....	1	.....	.....	114	114
1 673	Gadsden .....	1	.....	.....	110	110
2 687-A	Lake .....	1	120	.....	.....	120
4 640-B	Martin .....	1	132	.....	.....	132
5 625	Citrus .....	1	130	.....	.....	130
5 625	Citrus .....	1	.....	.....	94	94
5 682	Citrus .....	1	126	120	.....	246
6 670	Jackson .....	1	.....	.....	110	110
14 50-A	Putnam .....	1	120	.....	.....	120
33 652	Okaloosa .....	.....	.....	.....	120	120
Totals .....		195	11,911	1,714	15,526	29,151

STATE ROAD DEPARTMENT OF FLORIDA  
BRIDGES COMPLETED DECEMBER 31st, 1926.

Road No.	Project No.	COUNTY	Length Feet	TYPE	Total Cost	County Participation
1	3	Gadsden-Jackson .....	2,627	Concrete .....	\$ 690,619.90	\$ 60,000.00
2	7-B	Hamilton .....	476	Concrete and Steel .....	174,078.24	61,025.11
1	14-B	Santa Rosa .....	370	Steel .....	167,955.01	.....
2	19-B	Hamilton .....	106	Concrete .....	20,359.37	8,150.00
1	21-B	Baker .....	340	Concrete .....	40,778.78	.....
5	29-A	Hillsboro .....	316	Concrete .....	69,890.79	.....
5	29-B	Hillsboro .....	215	Concrete .....	83,455.47	.....
2	30	Columbia-Hamilton .....	325	Concrete .....	61,996.95	21,370.00
2	37-D	Alachua .....	60	Concrete .....	34,494.40	.....
1	38*	Escambia .....	18,470	Embankment-Concrete-Steel-Timber .....	987,236.47	100,000.00
4	39†	Brevard .....	2,221	Concrete and Embankment .....	233,975.43	.....
1	509	Gadsden .....	115	Timber .....	4,845.98	.....
1	517	Jefferson .....	90	Timber .....	4,564.13	.....
15	526	Pinellas .....	2,500	Concrete .....	251,902.22	.....
17	527	Pinellas .....	2,833	Concrete .....	162,297.03	.....
4	559	Brevard .....	260	Concrete .....	10,000.00	.....
8	562-A	Highlands .....	375	Timber .....	6,425.75	.....
2	563	Osceola .....	315	Timber .....	9,456.02	.....
2	566	Lake .....	266	Timber .....	6,493.83	.....
1	568	Jackson .....	3,315	Timber .....	109,552.65	.....
8	582	Okeechobee .....	3,125	Timber and Steel .....	167,929.12	.....

5	583	Hillsboro .....	170	Concrete .....	64,179.84	.....
1	20	Holmes .....	2,473	Timber and Steel .....	345,747.75	50,000.00
4	36-A	Indian River .....	72	Concrete .....	14,558.70	.....
4	601	St. Johns .....	480	Timber .....	29,274.37	.....
5	613	Sarasota .....	32	Concrete .....	12,839.65	12,839.65
4	620	Indian River .....	45	Timber .....	4,354.58	.....
1	631-B	Holmes .....	1,480	Timber .....	47,948.39	.....
1	633-B	Gadsden .....	120	Concrete .....	22,857.88	.....
1	634-B	Jackson .....	456	Concrete and Steel .....	31,577.51	.....
1	658-B	Washington .....	525	Timber .....	19,148.98	.....
3	660-B	Clay .....	225	Timber .....	12,054.71	.....
		Totals .....	44,798		\$ 3,902,849.90	\$ 313,384.76

\* 12305 feet of this is embankment.

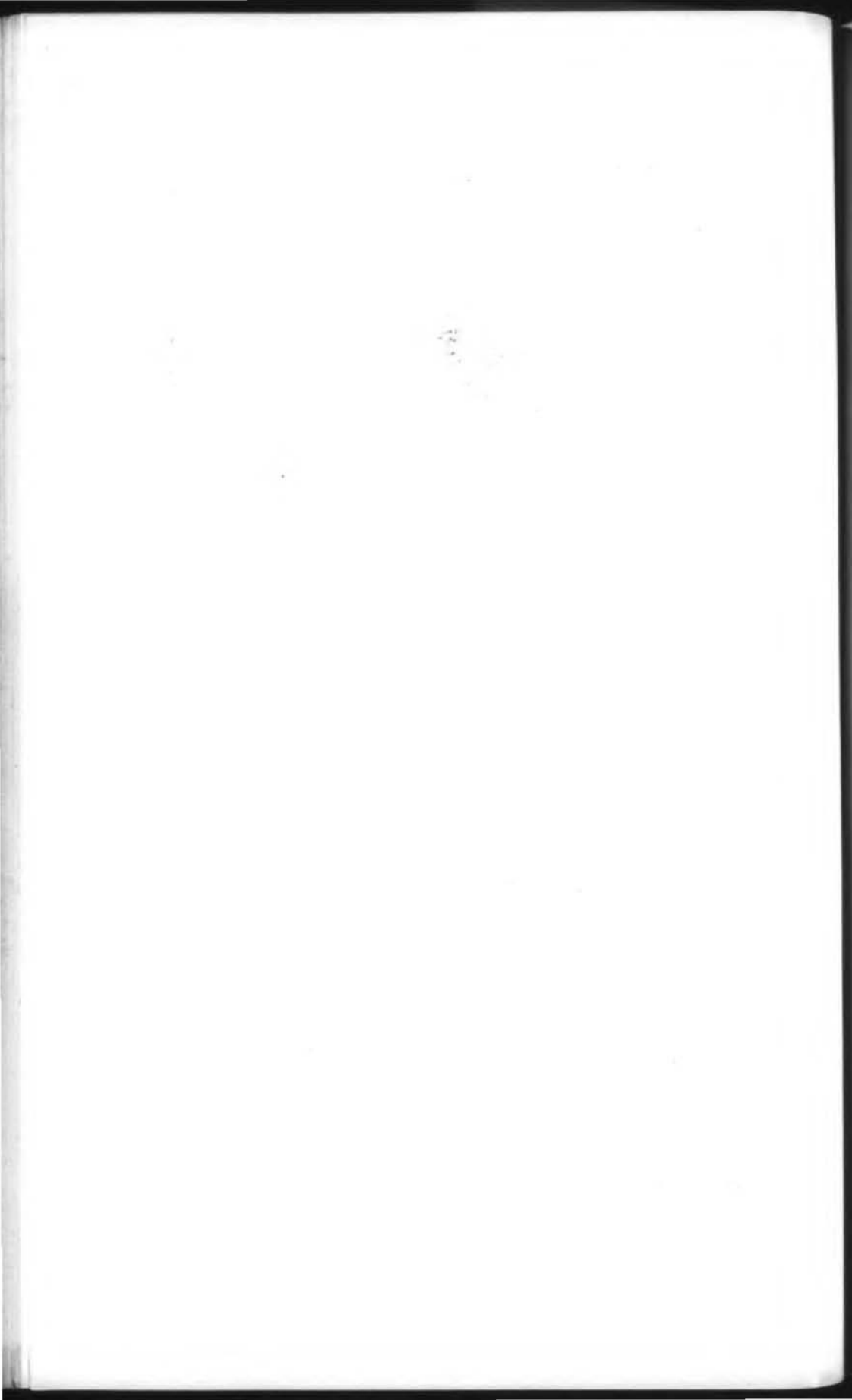
† 1320 feet of this is embankment.

STATE ROAD DEPARTMENT OF FLORIDA  
BRIDGES UNDER CONSTRUCTION DECEMBER 31st, 1926.

Road No.	Project No.	COUNTY	Length Feet	TYPE	Estimated Cost	Spent to Date	County Participation
1	45	Suwannee .....	910	Concrete and Steel ....	\$ 151,059.10	\$ 139,172.28	.....
4	48	St. Johns .....	456	Concrete .....	81,700.91	15,064.20	.....
4	49	Flagler .....	200	Concrete .....	62,967.30	22,056.68	.....
14	50-B	Putnam .....	150	Concrete .....	40,214.24	19,175.22	.....
3	421	Nassau .....	560	Concrete and Steel ....	233,094.18	231,501.86	.....
5	593	Manatee .....	3,600	Concrete .....	842,330.50	651,162.00	.....
1	634-B	Jackson .....	456	Concrete and Steel ....	70,707.86	31,577.51	.....
4	641-B	Palm Beach .....	297	Concrete and Steel ....	152,553.50	90,696.49	.....
10	645-B	Franklin-Wakulla .....	820	Timber .....	27,500.00	25,197.86	\$ 20,000.00
2	648-B	Hardee .....	615	Concrete-Steel-Timber ..	83,228.09	35,962.64	.....
24	534	Brevard .....	270	Timber .....	57,339.16	0.00	.....
1	672-B	Leon .....	1,950	Concrete and Steel ....	197,377.45	152,979.81	.....
1	673-B	Gadsden .....	478	Concrete .....	72,253.14	17,414.23	.....
1	621-B	Okaloosa .....	1,568	Concrete and Steel ....	157,361.60	48,719.62	.....
1	621-C	Okaloosa .....	1,363	Concrete and Steel ....	147,295.80	48,814.98	.....
1	621-D	Okaloosa .....	29	Concrete .....	19,068.28	7.50	.....
4	693	St. Lucie .....	160	Concrete .....	80,466.96	23,282.31	.....
4	694	Martin .....	327	Concrete .....	125,796.52	98,584.18	.....
4	656	Palm Beach .....	797	Concrete and Steel ....	253,987.30	108,075.11	.....
5	564-C	Charlotte .....	134	Steel .....	43,123.52	21,963.10	.....
		Totals .....	15,140		\$ 2,899,425.41	\$ 1,781,407.58	\$ 20,000.00



Looking West From Suwannee River Bridge. Road No. 1.



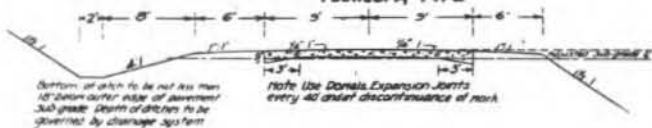
## ROAD CONSTRUCTION

During the 1923 session of the Legislature of Florida an Act was passed designating a definite system of roads and at the same time specified which of the roads might be built with the use of State funds. In 1925 this Act was amended but the same general policy was adhered to. A copy of this latter Act may be found in this report.

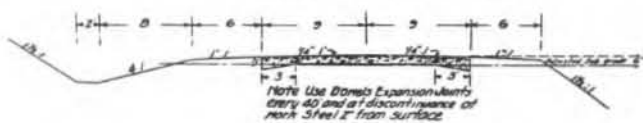
In the beginning of this biennium conditions for the building of highways in Florida were far from being favorable. Construction work of all kinds was at its peak, transportation facilities were overtaxed and labor was scarce as well as expensive. From January until June, 1925, this Department was by necessity forced to almost abandon the work of hard-surfacing, and as a result concentrated on the grading and drainage of certain projects. Conditions became more favorable during the latter part of 1925 and the work resumed its normal proportions. You will note from tabulations found in this report that during the past two years the Department has carried on quite an extensive road construction program, in grading, hard-surfacing and the building of bridges.

The specifications adopted by the State Road Department are of recognized standard and the results obtained are evidence of the care which is used in seeing that the State gets value received for the money spent. The building of roads in Florida presents many problems which must be reckoned with. The lowlands along the coast in certain localities, the great number of streams to be crossed and in many instances the unstable foundation. The progress of road building is sometimes very much retarded by local conditions and in many instances the most difficult work in connection with the construction of a bridge is under the ground or under water which sometimes causes the layman to believe that very little is being done as there is no visible indication of results obtained.

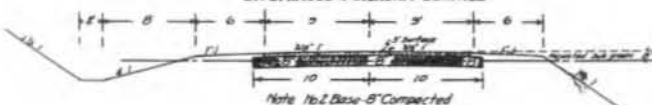
## TYPICAL CROSS SECTIONS

PLAIN CONCRETE SURFACE  
MARICOPA TYPE

## REINFORCED CONCRETE SURFACE



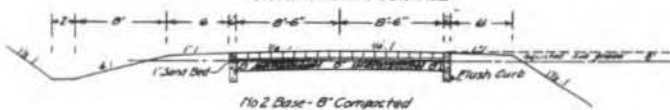
## BITUMINOUS MACADAM SURFACE



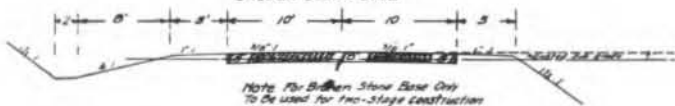
## SHEET ASPHALT OR BITUMINOUS CONCRETE



## VITRIFIED BRICK SURFACE



## BROKEN STONE BASE





The Department recognizes the great value of the road building materials found in this State and tries to make use of them at all times where conditions will warrant their use. Florida lime rock is used as a base under all asphaltic surfaces and it has proven to be equal to any other base yet devised for this purpose. The various tables in this report will show clearly the length, type and location of the various sections of road constructed, and the cut headed "Typical Cross Sections" will show the specification of each type as far as width and thickness is concerned.

STATE ROAD DEPARTMENT OF FLORIDA  
ROAD PROJECTS COMPLETED TO DECEMBER 31st, 1926

Road No.	Project No.	County	Length Miles	Type	Cost	* County Participation
2	2	Osceola .....	4.13	Brick .....	\$ 125,693.90	\$ 15,783.38
2	4	DeSoto .....	3.03	Concrete .....	92,576.69	17,017.12
18	5	DeSoto .....	2.50	Concrete .....	50,431.49	12,042.38
1	6	Madison .....	5.60	Sand Clay .....	45,334.31	2,500.00
2	8	Alachua .....	11.73	Bituminous Macadam .....	250,512.42	49,918.10
1	9	Holmes .....	5.11	Sand Clay .....	50,611.12	9,987.49
1	11	Baker, Nassau and Duval .....	15.39	Concrete .....	451,805.00	19,393.03
2	13	Orange .....	8.59	Sheet Asphalt .....	287,517.51	.....
1	14	Santa Rosa .....	6.02	Brick .....	135,522.26	64,546.27
2	15	DeSoto .....	3.28	Concrete .....	70,356.13	16,000.00
18	16	DeSoto .....	9.17	Concrete .....	127,133.45	95,680.62
5	17	Manatee .....	7.07	Bituminous Macadam .....	202,712.75	100,000.00
3	18	Putnam .....	12.80	Bituminous Macadam .....	456,110.33	143,543.42
2	19	Hamilton .....	8.20	Broken Stone Base .....	220,011.27	21,600.00
1	21	Baker .....	5.68	Concrete .....	171,713.65	19,393.02
1	22	Baker .....	8.90	Concrete .....	250,774.05	16,333.03
1	23	Duval .....	12.80	Concrete .....	440,646.88	.....
1	24	Columbia .....	9.89	Bituminous Macadam .....	295,706.67	88,700.00
2	26	Columbia .....	11.01	Concrete .....	377,386.39	17,690.00
2	27	Columbia .....	12.36	Concrete .....	516,981.98	154,800.00
2	31	Hamilton .....	11.86	Broken Stone Base .....	353,704.31	32,825.00
4	32	Nassau .....	10.00	Bituminous Macadam .....	367,133.76	.....
2	33	Alachua .....	12.60	Bituminous Macadam .....	373,977.67	177,467.14
7	34	Escambia .....	10.00	Concrete .....	285,409.07	139,901.08

1	35	Escambia	5.00	Concrete	148,494.40	84,950.54
4	36-A	Indian River	7.76	Bituminous Macadam	259,148.16	51,450.00
4	36-B	Indian River	7.12	Concrete	248,228.20	51,450.00
2	36-A-C	Alachua	3.96	Sheet Asphalt	171,145.32	85,572.66
2	37-E	Alachua	7.96	Sheet Asphalt	328,977.98	164,488.99
4	40-A	Brevard	16.17	Broken Stone Base	619,577.71	87,318.00
4	40-D	Brevard	6.72	Broken Stone Base	196,913.77	36,288.00
4	40-E	Brevard	13.60	Broken Stone Base	355,597.16	73,440.00
2	43	Marion	10.40	Sheet Asphalt	364,238.39	.....
2	44	Lake	10.52	Bituminous Concrete	387,044.94	269,350.00
7	51	Escambia	6.3	Concrete	194,139.10	50,000.00
20	500	Bay	35.00	Sand Clay	150,174.81	.....
6	501	Calhoun and Gulf	44.00	Sand Clay	125,038.30	50,739.96
6	502	Gulf	7.50	Sand Clay	47,210.59	21,770.40
2	503	Charlotte	20.18	Marl	141,530.13	.....
1	504	Columbia	9.15	Broken Stone Base	222,227.32	14,460.00
2	505	Columbia	11.80	Broken Stone Base	217,864.19	65,350.00
4	507	Flagler	9.00	Broken Stone Base	240,123.78	28,500.00
5	510	Hillsboro	9.20	Asphalt Block	255,976.44	.....
5	511	Hillsboro	11.00	Asphalt Block	140,468.85	.....
5	512	Hillsboro	3.00	Asphalt Block	21,492.56	.....
1	515	Jackson	16.60	Sand Clay	114,508.19	.....
5-A	518	Lafayette	15.00	Sand Clay	6,452.17	.....
5	519	Manatee	3.50	Bituminous Macadam	105,314.66	.....
2	520	Marion	15.53	Broken Stone Base	255,532.58	176,755.76
4	521	Nassau	12.41	Broken Stone Base	467,942.06	.....
8	523	Okeechobee	18.46	Broken Stone Base	440,873.78	209,605.02
5-A	531*	Taylor	8.00	Sand Clay	941.75	.....
3	532	Volusia	18.90	Broken Stone Base	332,255.78	133,322.27
5-A	535*	Lafayette	8.00	Sand Clay	6,452.17	.....
1	536	Gadsden	5.50	Sand Clay	31,452.82	10,000.00

STATE ROAD DEPARTMENT OF FLORIDA  
ROAD PROJECTS COMPLETED TO DECEMBER 31st, 1926—(Continued)

Road No.	Project No.	County	Length Miles	Type	Cost	County Participation
23	537*	Marion .....	6.93	Broken Stone Base .....	\$ 15,462.00	.....
2	538	Marion .....	10.92	Broken Stone Base .....	181,227.44	\$113,210.18
5	539	Marion .....	11.30	Broken Stone Base .....	87,624.13	66,150.85
5	544	Pasco .....	20.08	Broken Stone Base .....	457,778.80	.....
5	545	Hernando .....	9.51	Sheet Asphalt .....	303,782.81	100,000.00
4	546	Brevard .....	5.00	Broken Stone Base .....	124,347.73	25,469.09
5	548*	Levy .....	20.06	Surface Treatment .....	46,462.02	.....
2	553	Marion .....	9.15	Broken Stone Base .....	135,809.52	83,194.36
4	554	Brevard .....	5.00	Broken Stone Base .....	121,158.52	25,496.08
1	557	Gadsden .....	.06	Sand Clay .....	2,100.00	.....
6	560	Gulf .....	20.00	Sand Clay .....	82,474.00	29,826.90
8	562-A	Highlands .....	5.37	Broken Stone Base .....	139,552.57	.....
8	562-B	Highlands .....	10.71	Sheet Asphalt .....	350,579.99	.....
5	564-A*	Charlotte .....	10.88	Broken Stone Base .....	203,830.43	45,000.00
1	567	Walton .....	21.35	Sand Clay .....	122,528.30	.....
5	570	Manatee .....	3.96	Bituminous Macadam .....	150,391.74	.....
13	572	Bradford .....	7.50	Broken Stone Base .....	138,065.62	92,421.88
9	574	Madison .....	11.66	Broken Stone Base .....	283,342.57	75,269.65
3	575	Putnam .....	5.46	Broken Stone Base .....	180,574.00	54,172.47
5	576	Sarasota .....	5.68	Broken Stone Base .....	176,086.37	.....
1	579	Holmes .....	8.62	Sand Clay .....	88,091.04	5,012.00
5	581	Hillsboro .....	12.10	Broken Stone Base .....	367,655.88	.....
24	584	Osceola .....	2.00	Brick .....	45,608.33	.....
1	586	Jackson and Washington .....	17.37	Sand Clay .....	149,076.77	.....

3	588	Putnam	2.34	Sheet Asphalt	61,579.18	
13	594	Bradford	9.10	Broken Stone Base	128,870.93	170,770.28
3	595	Volusia	7.43	Broken Stone Base	162,374.17	35,000.00
4	597	Volusia	16.24	Broken Stone Base	511,684.07	149,100.00
1	598	Jefferson	17.25	Sand Clay	215,617.68	
2	599	DeSoto	7.40	Marl	49,271.01	19,362.64
4	604	Volusia	7.72	Broken Stone Base	290,254.23	70,900.00
13	607	Bradford	5.10	Broken Stone Base	88,624.25	44,561.39
13	607-B	Clay	7.00	Broken Stone Base	146,577.24	
4	608	Brevard	9.29	Concrete	372,748.39	51,961.83
1	612	Leon	17.58	Sand Clay	171,956.54	
5	613	Sarasota	4.62	Sheet Asphalt	290,053.53	117,746.74
5	617	Alachua	5.17	Broken Stone Base	101,560.45	55,363.37
5	618	Alachua	10.89	Broken Stone Base	212,137.99	110,726.74
2	622	Lake	.21	Bituminous Concrete	7,283.76	8,217.80
5	625	Citrus	10.86	Broken Stone Base	199,379.62	170,197.72
5	626	Citrus	6.61	Broken Stone Base	111,887.45	95,925.68
3	627	Putnam	6.27	Broken Stone Base	171,526.56	62,209.04
3	628	Volusia	9.92	Broken Stone Base	239,743.95	100,000.00
8	629	Highlands	6.00	Broken Stone Base	152,744.83	
8	630	Highlands	11.00	Broken Stone Base	237,021.37	
1	631	Washington	8.53	Sand Clay	154,793.48	
1	633	Gadsden	9.67	Sand Clay	118,916.58	
1	634	Jackson	11.15	Sand Clay	138,376.92	
5	635	Manatee	1.28	Bituminous Macadam	28,920.08	
8	636	St. Lucie	12.80	Broken Stone Base	296,495.20	76,000.00
10	637	Leon	18.08	Sand Clay	123,800.83	
1	639	Gadsden	9.83	Sand Clay	82,440.65	
3	642	Putnam	10.82	Broken Stone Base	288,650.60	107,352.77
10	644	Wakulla	8.49	Graded	63,620.81	
8	647	Highlands	7.00	Broken Stone Base	149,290.69	

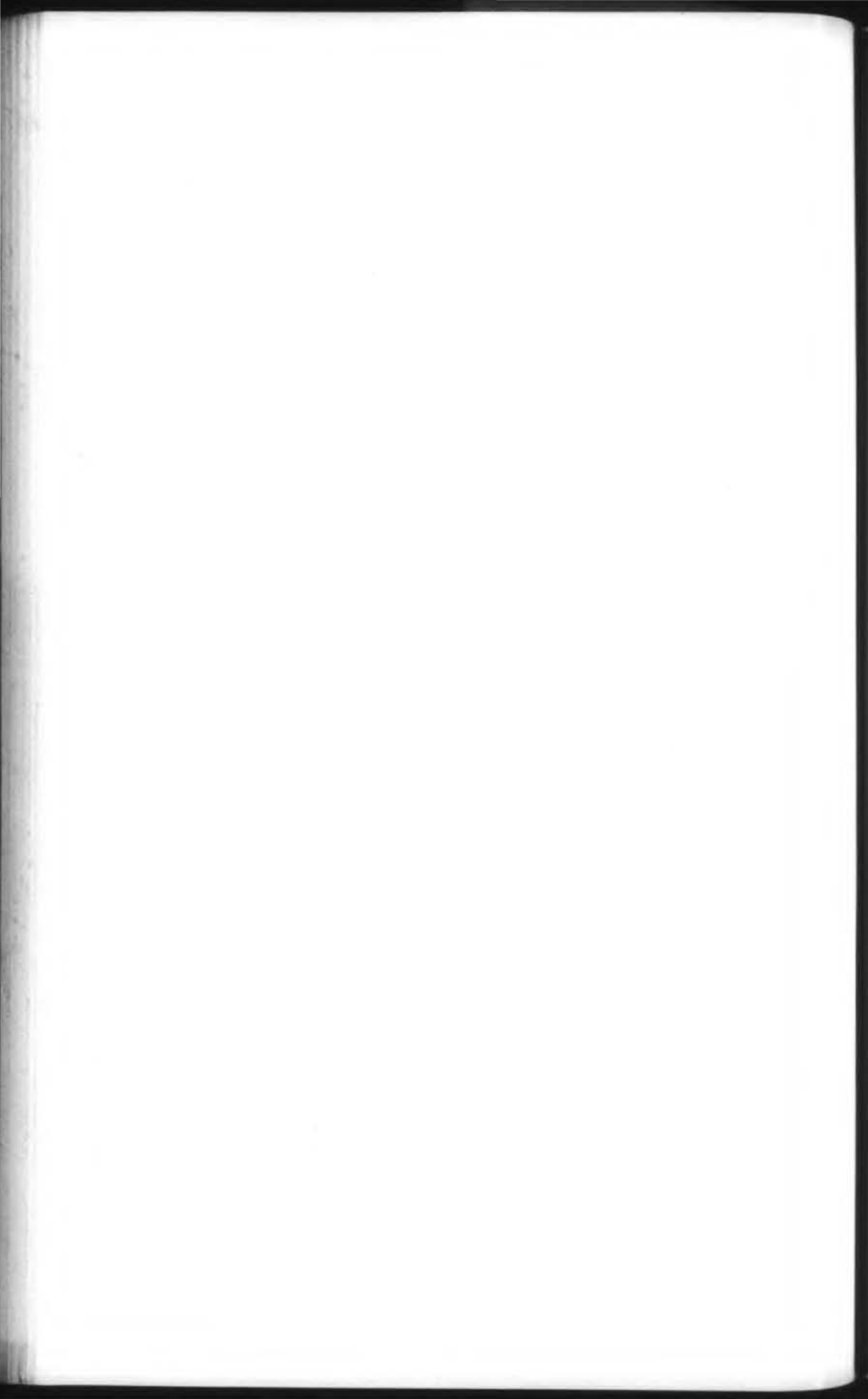
STATE ROAD DEPARTMENT OF FLORIDA  
ROAD PROJECTS COMPLETED TO DECEMBER 31st, 1926—(Continued)

Road No.	Project No.	County	Length Miles	Type	Cost	County Participation
33	649	Okaloosa .....	10.43	Sand Clay .....	\$ 37,393.99	\$ 37,393.99
33	652	Okaloosa .....	9.04	Sand Clay .....	59,070.88	59,070.88
1	658	Holmes .....	8.21	Sand Clay .....	84,997.27	.....
2	661	Lake .....	3.52	Sheet Asphalt .....	165,893.83	88,000.00
6	666	Jackson .....	7.00	Sand Clay .....	65,335.29	.....
6	670	Jackson .....	12.30	Sand Clay .....	83,568.49	.....
1	672	Leon .....	9.92	Sand Clay .....	117,524.19	.....
5-A	689	Okaloosa .....	1.67	Broken Stone Base .....	32,196.82	.....
Total .....			1,158.41		\$22,565,164.63	\$ 5,162,026.91

\* County Participated in work handling its Finances.



Typical Section of Florida Highway.





STATE ROAD DEPARTMENT OF FLORIDA  
ROAD PROJECTS UNDER CONSTRUCTION

Road No.	Project No.	County	Length Miles	Type	Estimated Cost	Amount Spent to Date	County Participation
1	1	Okaloosa .....	10.40	Sand-Clay .....	\$ 113,962.84	\$ 33,962.84	\$ 9,989.40
2	7	Hamilton .....	12.90	Broken Stone Base ....	455,862.55	295,563.26	20,425.00
1	14	Santa Rosa .....	6.00	Concrete Shoulders ....	226,719.51	155,604.67	64,546.27
2	37-D	Alachua .....	2.14	Broken Stone Base ....	205,492.09	183,474.85	72,876.93
4	41	Dade .....	11.36	Sheet Asphalt .....	692,319.40	554,558.75	.....
3	46	Nassau .....	11.51	Concrete .....	495,071.06	208,922.82	.....
4	47	St. Johns .....	14.96	Concrete .....	573,849.99	168,029.15	.....
4	48	St. Johns .....	15.39	Grading .....	161,619.08	172,724.09	.....
4	49	Flagler .....	13.81	Grading .....	123,136.42	98,575.59	.....
14	50-A	Putnam .....	6.18	Grading .....	71,433.72	18,744.14	61,316.09
14	50-B	Putnam .....	9.77	Grading .....	111,313.18	89,354.30	96,934.98
14	50-C	Putnam .....	10.03	Grading .....	89,987.85	43,033.28	99,514.65
2	53-A	Lake .....	7.09	Bituminous Concrete ..	60,252.65	61,841.98	177,250.00
2	53-C	Lake .....	1.87	Grading .....	113,087.04	2,515.55	46,750.00
20	500-A	Bay .....	9.65	Concrete .....	334,611.59	193,072.20	284,691.60
1	514	Jackson .....	11.04	Sand-Clay .....	82,000.00	70,068.99	.....
1	529	Suwannee .....	12.70	Concrete .....	440,444.52	134,599.66	.....
1	533	Suwannee .....	13.47	Broken Stone Base ....	341,475.19	235,366.80	.....
24	534	Brevard .....	14.10	Broken Stone Base ....	652,293.26	598,387.84	120,000.00
3	543	Seminole .....	14.20	Grading .....	109,769.62	86,077.51	.....
5	564-B	Charlotte .....	9.73	Broken Stone Base ....	341,946.19	302,773.20	50,000.00
5	564-C	Charlotte .....	3.93	Grading .....	45,269.98	42,938.56	.....
1	565	Madison .....	15.99	Concrete .....	558,893.36	280,693.14	131,000.00

STATE ROAD DEPARTMENT OF FLORIDA  
ROAD PROJECTS UNDER CONSTRUCTION—(Continued)

Road No.	Project No.	County	Length Miles	Type	Estimated Cost	Amount Spent to Date	County Participation
1	571	Madison .....	14.26	Broken Stone Base....	\$ 347,865.45	\$ 171,398.69	\$ 110,000.00
19	580	Dixie .....	13.00	Broken Stone Base ....	230,000.00	111,342.01	100,000.00
5-A	587	Columbia .....	4.37	Grading .....	27,176.71	1,715.35	27,176.71
5	589	Lee .....	8.27	Broken Stone Base ....	237,423.52	193,118.83	.....
1	590	Santa Rosa .....	9.00	Grading .....	79,250.00	38,879.18	.....
10	592	Franklin .....	7.89	Grading .....	47,765.75	4,580.00	146,768.65
2	609	Hardee .....	1.51	Broken Stone Base ....	24,504.19	24,504.19	24,504.19
5	614	Sarasota .....	19.08	Grading .....	202,576.25	182,971.52	.....
5	619	Alachua .....	9.28	Grading .....	42,957.36	12,997.19	180,000.00
1	621	Okaloosa .....	17.35	Grading .....	158,795.00	139,327.27	.....
35	623	Madison .....	12.13	Grading .....	110,000.00	94,202.05	.....
1	634	Jackson .....	11.15	Grading .....	145,000.00	138,376.92	.....
3	640-A	Martin .....	9.00	Grading .....	81,113.16	5,287.77	.....
4	640-B	Martin .....	11.80	Grading .....	125,571.53	7,133.15	.....
4	641-A	Palm Beach .....	11.36	Grading .....	86,081.38	68,059.80	.....
10	645	Wakulla .....	17.00	Shell .....	204,000.00	100,491.27	100,000.00
10	646	Franklin .....	18.00	Shell .....	216,000.00	177,997.44	.....
2	648	Hardee .....	14.19	Broken Stone Base ....	191,104.49	106,959.95	.....
10	651	Gulf .....	14.72	Grading .....	120,000.00	90,960.34	32,677.30
4	653	Broward .....	13.46	Grading .....	75,976.29	9,240.70	.....
18	655	Highlands .....	15.00	Broken Stone Base ....	300,000.00	293,659.75	.....
6	657	Jackson .....	10.00	Grading .....	70,000.00	45,535.34	.....

3	659	Clay	13.27	Grading	133,334.45	41,918.21	47,750.00
3	660	Clay	10.52	Grading	250,116.89	123,126.17	47,750.00
5	663	Citrus	8.03	Broken Stone Base	224,291.06	189,549.50	
18	667	DeSoto	8.63	Broken Stone Base	105,874.01	54,999.18	
4	668	Brevard	13.45	Grading	119,128.58	82,010.09	
27	669-B	Dade	10.00	Broken Stone Base	430,500.00	412,711.23	61,000.00
27	669-C	Dade	12.00	Grading	311,027.69	56,090.12	64,000.00
27	669-V	Collier	11.91	Grading	382,631.70	138,367.89	
27	669-W	Collier	14.00	Broken Stone Base	280,000.00	82,228.53	
27	669-X	Collier	9.39	Grading	104,164.50	54,192.70	
1	673	Gadsden	9.90	Grading	120,000.00	108,090.48	
1	674	Duval	3.50	Concrete	127,109.00	957.31	
17	675	Polk	5.16	Sheet Asphalt	256,969.88	205,393.56	
19	676-A	Levy	9.95	Broken Stone Base	191,229.70	48,627.71	184,134.88
19	676-B	Levy	14.39	Broken Stone Base	341,693.10	126,484.77	341,693.10
19	676-C	Levy	13.93	Grading	93,096.41	35,084.04	93,096.41
13	677-A	Levy	6.96	Grading	38,450.94	13,908.88	38,450.94
13	677-B	Levy	11.58	Grading	161,287.36	9,160.33	161,298.36
5	679	Hernando	7.11	Broken Stone Base	202,092.75	181,542.98	
5	682	Citrus	6.45	Broken Stone Base	188,277.89	175,227.19	22,061.09
2	687-A	Lake	15.00	Grading	120,043.11	79,871.67	300,000.00
4	691	Indian River	5.52	Grading	95,138.91	14,319.70	
4	692	St. Lucie	7.38	Grading	130,489.86	14,851.56	
4	693	St. Lucie	8.73	Grading	90,372.00	80,066.09	200,000.00
4	694	Martin-St. Lucie	8.48	Grading	61,595.05	56,600.77	
19	698	Leon	12.43	Grading	153,687.30	5,475.61	153,687.30
19	699	Jefferson	7.70	Grading	45,000.00	6,590.25	57,750.00
5-A	718	Columbia	8.22	Grading	69,487.00	12,499.16	69,487.00
3	721	Putnam	4.00	Broken Stone Base	72,000.00	33,681.37	
Totals			777.63		\$14,423,141.31	\$ 8,493,284.93	\$ 3,798,569.85

# CONTRACTS AWARDED BY STATE ROAD DEPARTMENT OF FLORIDA

From January 1, 1926 to December 31, 1926

Rd. No.	Project No.	County	Contractor	Leng'h Miles	Length Feet	Contract Cost + 10%	Type
7	51	Escambia	Noonan-Lawrence	6.28	.....	\$ 286,595.89	Concrete
2	648-B	Hardee	J. S. Walton & Co.	.....	615	83,228.09	Concrete Timber
4	641-A	Palm Beach	Chas. F. Wilmore	11.36	.....	86,081.38	Grading
10	644-A	Wakulla	Gillis Construction Co.	8.49	.....	47,367.49	Grading
5	617-618	Alachua	L. M. Gray	16.06	.....	248,123.10	Rock Base
1	633-B	Gadsden	Montgomery & Parker	.....	240	39,591.47	Concrete Bridge
1	672-B	Leon	Peterson & Earnhart	.....	1950	197,377.45	Concrete Bridge
4	641-B	Palm Beach	Concrete Steel Bridge Co.	.....	237	120,766.80	Concrete Bridge
4	656	Palm Beach	Concrete Steel Bridge Co.	.....	727	222,200.60	Concrete Bridge
4	641-656	Palm Beach	American Bascule Bridge Corp.	.....	120	63,573.40	Bascules
5	689	Alachua	J. W. Hall	1.67	.....	10,599.42	Grading
8	562-A	Highlands	H. E. Wolfe	5.37	.....	142,837.33	Surface Treatment
4	694-694-A	Martin-St. Lucie	C. A. Steed & Son	8.48	.....	61,599.45	Grading
4	693	St. Lucie	Boone & Wester	8.73	.....	59,670.88	Grading
2	31	Hamilton	Duval Engineering & Contracting Co.	11.82	.....	241,489.11	Rock Base
3	642	Putnam	F. S. Whitney	10.18	.....	210,025.00	Rock Base
17	675	Polk	Wm. P. McDonald Construction Co.	5.16	.....	256,969.88	Sheet Asphalt
20	500-A	Bay	Noonan-Lawrence	9.65	.....	334,691.65	Concrete
4	693	St. Lucie	R. H. H. Blackwell	.....	160	80,466.96	Concrete
4	694-694-A	Martin-St. Lucie	C. A. Steed & Son	.....	327	125,796.52	Concrete
2	648	Hardee	Wm. P. McDonald Const. Co.	1.00	.....	31,363.20	Rock Base Sur- face Treated
19	676-A	Levy	Gilbert & Hadsock	9.95	.....	48,875.58	Grading
19	676-B	Levy	McLeod Construction Co.	14.39	.....	135,259.30	Grading
1	673	Gadsden	Peterson & Earnhart	.....	20	10,727.20	Concrete

1	38	Escambia	Atlantic Bridge Company	.....	28,260.98	Surface
3	543	Seminole	Lake Worth Construction Co.	14.02 .....	109,759.62	Grading
1	621-C	Okaloosa	A. Bentley & Sons Co.	..... 1363	147,295.80	Concrete
1	621-B	Okaloosa	A. Bentley & Sons Co.	..... 1568	157,361.60	Concrete
4	668	Brevard	E. F. Powers Const. Co.	13.45 .....	119,128.58	Grading
5	564-C	Charlotte	C. F. Powers Construction Co.	3.93 .....	42,264.98	Grading
3	595	Volusia	L. M. Gray	6.43 .....	122,022.17	Rock Base
2	53-A	Lake	B. Booth & Company	7.09 .....	60,252.65	Grading
5	589	Charlotte	H. E. Wolfe	8.16 .....	174,725.21	Rock Base
5	614	Sarasota	W. J. Bryson Paving Co.	17.08 204	129,895.29	Grading
1	634-B	Jackson	Atlantic Bridge Co.	..... 456	70,707.86	Concrete
1	504	Columbia	Duval Engineering & Contracting Co.	9.41 .....	178,551.36	Rock Base
27	669-C	Dade	R. C. Huffman Const. Co.	12.00 .....	311,027.69	Grading
5	682	Citrus	L. B. McLeod Construction Co.	6.45 .....	106,241.56	Rock Base
5	564-C	Charlotte	Wilson Pipe Co.	..... 134	43,123.52	Concrete & Steel
3	659	Clay	M. C. Winterburn, Inc.	13.27 .....	133,334.45	Grading
18	655	Highlands	H. E. Wolfe	5.00 .....	105,874.01	Rock Base
5	564-B	Charlotte	Broadbent Construction Co.	9.73 .....	190,330.76	Rock Base
5	589	Charlotte	Thompson & Moseley	6.66 .....	10,916.86	Canal
5	663	Citrus	Wm. P. McDonald Const. Co.	8.03 .....	138,345.79	Rock Base
			Carried forward .....	262.82 7917	\$ 5,344,697.89	

CONTRACTS AWARDED BY STATE ROAD DEPARTMENT OF FLORIDA  
From January 1, 1926 to December 31, 1926

Rd. No.	Project No.	County	Contractor	Length Miles	Length Feet	Contract Cost + 10%	Type
4	48-B	St. Johns	Peterson & Earnhart	.....	456	\$ 81,700.91	Concrete Bridge
4	49-B	Falger	F. M. Stuart & Co.	.....	200	62,967.30	Concrete Bridge
1	533	Suwannee	Baker & Foulks	13.47	.....	249,410.72	Rock Base
27	669-V	Collier	Alexander, Ramsey & Kerr, Inc.	11.91	.....	382,631.70	Grading
27	669-X	Collier	Alexander, Ramsey & Kerr, Inc.	9.39	.....	104,164.50	Grading
10	592	Franklin	Bennett Construction Co.	7.89	.....	47,765.75	Grading
1	565	Madison	Johnson, Drake & Piper, Inc.	15.99	.....	464,271.36	Concrete
2	687-A	Lake	E. W. Ellis	15.00	.....	120,043.11	Grading
1	673-B	Gadsden	Peterson & Earnhart	.....	418	61,525.44	Concrete Bridge
13	677-A	Levy	A. J. Hoffman	6.96	.....	34,450.94	Grading
19	676-C	Levy	Langston Construction Co.	13.93	.....	93,096.41	Grading
1	14	Santa Rosa	Preskett, Patterson & Blackburn	6.00	.....	95,568.34	Concrete Shoulders
2	50-C	Putnam	E. Roy James	10.03	.....	89,350.31	Grading
2	7	Hamilton	Duval Engineering & Contracting Co.	12.90	.....	246,862.55	Rock Base
5	679	Hernando	Wm. P. McDonald Const. Co.	7.11	.....	129,358.28	Rock Base
2	53-C	Lake	Tampa, Sand & Shell Co.	1.87	.....	113,087.04	Grading
14	50-A	Putnam	Boone & Wester	6.18	.....	71,433.42	Grading
3	660	Clay	Langston Construction Co.	10.52	.....	180,619.71	Rock Base
5	619	Alachua	J. L. Gladwell	9.28	.....	42,957.36	Grading
1	529	Suwannee	M. C. Winterburn, Inc.	12.70	.....	365,381.55	Concrete
24	534	Brevard	M. C. Winterburn, Inc.	10.00	.....	132,405.26	Repair
5	589	Lee	J. M. Parker	8.27	.....	15,400.00	Shoulders
5	682	Citrus	The Erler Corporation	.....	247	60,573.90	Overhead
13	677-B	Levy	Coastal Construction Co.	11.58	.....	161,287.36	Grading
4	691	Indian River	Mason Payne Co., Inc.	5.52	.....	95,138.91	Grading

4	692	St. Lucie	Boone & Wester	7.38	.....	130,489.86	Grading
19	676-A	Levy	L. M. Gray	9.96	.....	142,354.12	Rock Base
19	676-B	Levy	L. M. Gray	14.39	.....	205,463.80	Rock Base
3	46	Nassau	W. J. Bryson Paving Co.	11.51	.....	337,808.87	Concrete
4	47	St. Johns	J. B. McCrary Engineering Corp.	14.96	.....	433,043.29	Concrete
1	674	Duval	Nelson Brothers	3.50	.....	127,109.00	Concrete
1	571	Madison	Duval Engineering & Contracting Co.	14.26	.....	236,789.17	Rock Base
2	37-D	Alachua	L. M. Gray	2.14	.....	32,594.07	Rock Base
2	648	Hardee	Wm. P. McDonald Const. Co.	7.14	.....	105,173.31	Rock Base
5-A	587	Columbia	C. G. Kershaw Const. Co.	4.37	.....	27,176.71	Grading
5-A	718	Columbia	C. G. Kershaw Const. Co.	8.22	.....	69,487.00	Grading
4	640-A	Martin	S. J. Groves & Sons Co.	9.00	.....	81,113.16	Grading
4	640-B	Martin	Lake Worth Construction Co.	11.80	.....	125,751.53	Grading
4	653	Broward	H. D. Spangler	13.46	.....	75,976.29	Grading
19	698	Leon	Curtis & Gubnins	12.43	.....	153,687.30	Grading
1	621-D	Okaloosa	Montgomery & Parker	.....	29	19,068.28	Concrete
5	682	Citrus	W. C. Shepherd	Raising fill	.....	12,375.00	Grading
5	564-B-589	Lee-Charlotte	H. E. Wolfe	.....	.....	60,348.14	Surface Treatment
Total .....				605.57	9276	\$11,226,958.92	

CONTRACTS AWARDED BY STATE ROAD DEPARTMENT OF FLORIDA  
From January 1, 1925, to December 31, 1925.

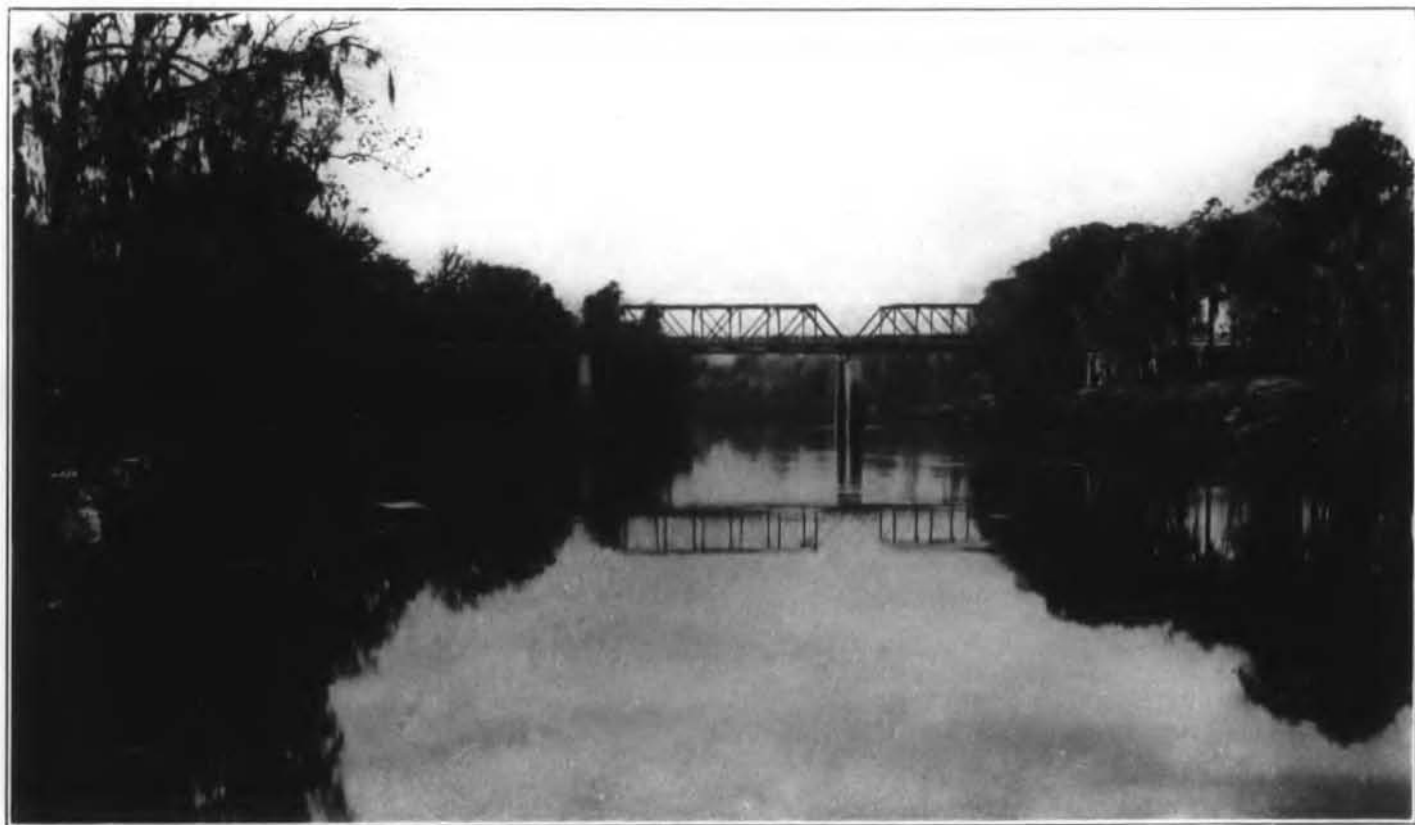
Rd. No.	Project No.	County	Contractor	Leng'h Miles	Length Feet	Contract Cost + 10%	Type
1	38-B	Escambia	Atlantic Bridge Company	.....	1570	\$ 289,113.77	Concrete and Steel
1	45	Madison	R. H. H. Blackwell	.....	910	124,902.27	Concrete and Steel
24	534	Brevard	Langston Construction Co.	7.69	.....	177,361.80	Rock Base
3	46	Nassau	B. Booth & Company	11.52	.....	95,664.77	Grading
3	421	Nassau	Pensacola Shipbuilding Co.	.....	760	233,094.18	Concrete and Steel
3	46	Nassau	Luten Bridge Company	.....	.....	52,150.60	Drainage Structures
5	581	Hillsborough	Luten Bridge Company	.....	.....	75,837.26	Drainage Structures
5	581	Hillsborough	Barnes & Smith	12.10	.....	240,237.78	Rock Base
4	47	St. Johns	Boone & Wester	14.96	.....	142,934.86	Grading & Drainage
8	647	Highlands	Otis Hardin	4.00	.....	16,720.00	Grading
3	607-B	Clay	Whitney Construction Co.	6.68	.....	89,200.32	Rock Base
5	617	Alachua	Taylor & Cox	5.17	.....	23,656.66	Grading & Drainage
5	618	Alachua	J. R. & J. B. Miller	10.89	.....	49,163.26	Grading & Drainage
3	628	Volusia	L. M. Gray	9.92	.....	152,099.00	Rock Base
8	629	Highlands	H. E. Wolfe	6.00	.....	.....	.....
8	647	Highlands	H. E. Wolfe	7.00	.....	156,113.44	Rock Base
1	638	Santa Rosa	S. G. Collins	.34	.....	16,170.00	Embankment
2	661	Lake	Southern Paving Construction Co.	3.52	.....	170,834.20	Sheet Asphalt
1	633	Gadsden	Taylor Contracting Co.	9.67	.....	61,077.46	Grading
1	639	Gadsden	Taylor Contracting Co.	9.83	.....	37,984.17	Grading
3	642	Putnam	B. Booth & Company	10.82	.....	66,840.94	Grading
33	649	Okaloosa	Penton-Mathis Construction Co.	10.43	.....	42,351.78	Sand Clay
33	652	Okaloosa	Penton-Mathis Construction Co.	9.04	.....	55,278.01	Sand Clay
5	613	Sarasota	Broadbent Construction Co.	4.62	.....	229,961.64	Sheet Asphalt
3	595	Volusia	J. L. Hunter	7.43	.....	28,382.46	Grading & Drainage



1	586	Jackson-Washington	Gillis Construction Co.	7.47	.....	39,767.20	Grading
5	625	Citrus	Ed Pettus	.....	94	3,509.00	Underpass
27	669-B	Dade	M. C. Winterburn, Inc.	10.30	.....	263,617.64	Rock Base
4	41	Dade	Morgan-Hill Paving Co.	11.76	.....	690,797.00	Sheet Asphalt
3	627	Putnam	L. M. Gray	6.27	.....	100,210.83	Rock Base
2	19	Hamilton	Duval Engineering & Contracting Co.	8.16	.....	176,439.17	Rock Base
4	48	St. Johns	A. J. Hoffman	15.39	.....	197,559.43	Grading & Drainage
4	49	Flagler	A. J. Hoffman	13.81	.....	130,171.16	Grading & Drainage
1	621	Okaloosa	Penton-Mathis Construction Co.	17.35	.....	113,547.16	Grading & Drainage
1	631	Holmes	E. P. Toulmin	8.53	.....	129,340.16	Grading & Drainage
2	648	Hardee	Federal Contracting Co.	14.19	.....	94,453.96	Grading & Drainage
18	655	Highlands	H. E. Wolfe	10.00	.....	165,668.14	Rock Base
1	658	Washington	Myers Construction Co.	8.21	.....	64,049.30	Grading & Drainage
5	663	Citrus	Taylor Contracting Co.	8.03	.....	85,945.26	Grading & Drainage
5	679	Hernando	Taylor Contracting Co.	7.11	.....	72,734.47	Grading & Drainage
5	682	Citrus	Caye-Andrews Construction Co.	6.45	.....	82,030.82	Grading & Drainage
14	50-B	Putnam	Luten Bridge Company	.....	150	40,214.24	Concrete Bridge
5	625	Citrus	R. H. H. Blackwell	.....	130	25,551.24	Concrete Overhead
1	631-B	Holmes	F. D. Harvey & Co.	.....	1480	48,528.26	Timber Bridge
1	658-B	Washington	F. D. Harvey & Co.	.....	525	19,458.78	Timber Bridge
14	50-B	Putnam	B. Booth & Company	9.77	.....	101,165.34	Grading
3	660	Clay	B. Booth & Company	10.52	.....	69,497.18	Grading
Totals .....				334.95	5419	\$ 5,341,199.87	

STATE ROAD DEPARTMENT OF FLORIDA  
OVER PASSES COMPLETED

Road No.	Project No.	County	Type	Length Feet	Cost	Railroad
1	54	Leon .....	Timber	110.0	3,482.52	Atlantic Coast Line
1	536	Gadsden .....	Timber	185.0	5,338.09	Louisville & Nashville
1	565	Suwannee .....	Timber	108.0	3,312.17	Seaboard Air Line
1	567	Walton .....	Timber	77.0	1,370.44	Louisville & Nashville
1	567	Walton .....	Timber	77.0	1,264.21	Louisville & Nashville
1	579	Holmes .....	Timber	110.0	6,160.90	Louisville & Nashville
1	621	Okaloosa .....	Timber	115.0	5,485.16	Louisville & Nashville
1	633	Gadsden .....	Concrete	120.0	18,455.75	Louisville & Nashville
1	673	Gadsden .....	Timber	111.0	6,195.95	Georgia, Florida & Alabama
2	43	Marion .....	Concrete	120.0	16,601.80	Atlantic Coast Line
5	545	Hernando .....	Timber	183.0	8,280.56	Seaboard Air Line
5	625	Citrus .....	Timber	94.0	3,227.04	Seaboard Air Line
5	625	Citrus .....	Concrete	130.0	24,875.99	Atlantic Coast Line
6	670	Jackson .....	Timber	111.0	5,087.31	Atlantic & St. Andrews Bay
33	652	Okaloosa .....	Timber	113.0	4,635.01	Louisville & Nashville
		Total .....	.....	1,764.0	\$113,772.90	



Suwannee River Bridge.



STATE ROAD DEPARTMENT OF FLORIDA  
OVER PASSES UNDER CONSTRUCTION

Road No.	Project No.	County	Type	Length Feet	Estim'ted Cost	Amount Spent	Railroad
5	682	Citrus .....	Concrete & Steel	247.0	\$64,186.40	\$.....	S. A. L. and A. C. L.
6	657	Jackson .....	Timber	114.0	5,000.00	4,642.13	Mariana & Blountstown
		Total .....		361.0	\$69,186.40	\$4,642.13	

NOTE: Three Under Passes have been completed and one now building. Railroad is constructing and bills have not yet been submitted to the Department.

## MAINTENANCE.

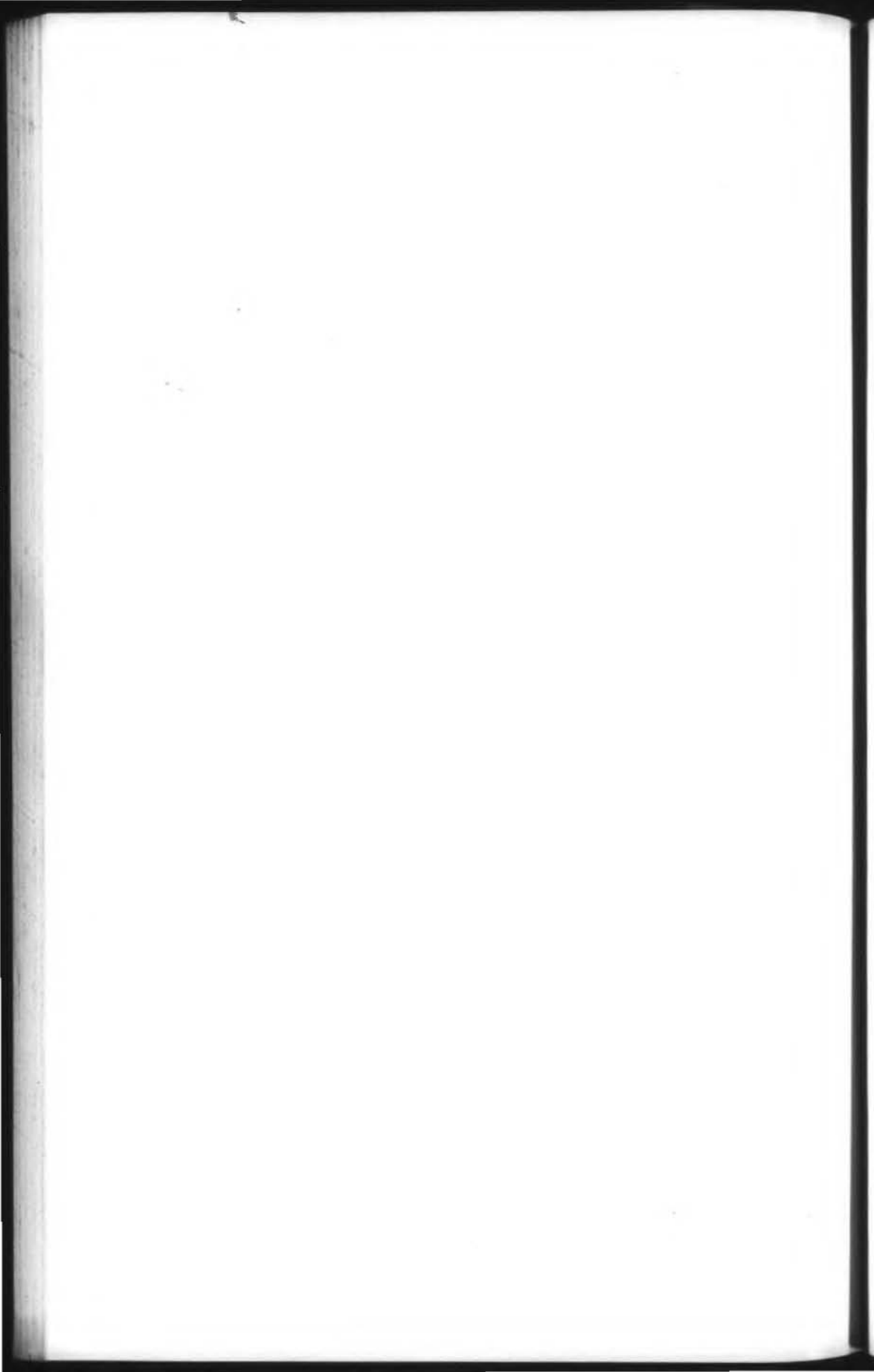
Maintenance is one of the problems of any highway department and represents one of its major operations. This department several years ago recognized the importance of taking care of what it had built and improving the same. This policy has been improved upon with each administration and as more roads were constructed the organization became larger and more efficient.

Efficient highway maintenance gives the public as much value per dollar as any other form of expenditure of public funds and the first call on highway resources should be for the proper maintenance of existing roads of the trunk line system and thereby protect the public's investment in these roads. The department has been following what is known as the patrol system of maintenance upon its roads. This system is similar to that employed by railroads where they divide their lines into sections and upon each section place a section foreman. All main roads in the system where maintenance is carried on are divided into sections and upon each section is placed a foreman who is supplied with the necessary equipment, such as truck, small tools, grader, tractor, or whatever is necessary to properly maintain the type of road of his section. The class of equipment and the length of a section depend largely upon the type of surface of the road, as the higher the type of surface, the greater the length of section, the lighter the equipment, and, therefore, the less cost per mile, and vice versa.

This department fully realizes the necessity of maintaining the roads it constructs, which includes the taking care of all the roads constructed by it or such roads on the primary system that the department has taken over and placed under its supervision. It is the common belief that in most cases the cost of maintaining any road



Aerial View, Manatee River Bridge. Road No. 5.





is in proportion to the travel the road carries, but this is not always true. The cost of maintaining a road surface is about inversely in proportion to the cost of the surface to be maintained; that is, the cheaper the surface, the higher the maintenance cost. One of the largest maintenance charges on the hardsurfaced roads in Florida is the sodding of shoulders and in the protection of same until grass has gotten such a start as not to be destroyed by rains or by cattle, and the keeping of the drainage ditches thoroughly open so that they will function at all times. These drainage ditches include not only the parallel ditches but the outlet ditches as well, some of which extend quite a distance away from the roads. The department feeling that no road is properly constructed that is not thoroughly drained and to properly drain same, ditches leading away from the road are constructed.

On sand clay roads the grass shoulders are not so essential, therefore, the greater cost is in keeping the surface machined and dragged, as well as the ditches open; also the addition of such quantities of clay and sand as will replace that part either blown away in dust or washed off the road by heavy rains, trying at all times to maintain the same type and quantity of material in the surface as that of the road when it was first completed.

For the past four years this department has been carrying on its maintenance work on a budget system and found this to work very well and the cost for the year has conformed very closely to the budget prepared. In 1923 a budget was prepared for the maintenance of 550 miles of road; in 1924 to cover the maintenance of 898 miles of road; for the year 1925 to cover 1,275 miles of road. For 1926 the budget prepared was for 1,651 miles and for the year 1927 the budget which has been prepared covers the maintenance of 2,104 miles of road. From this it will be noted that there has been a con-

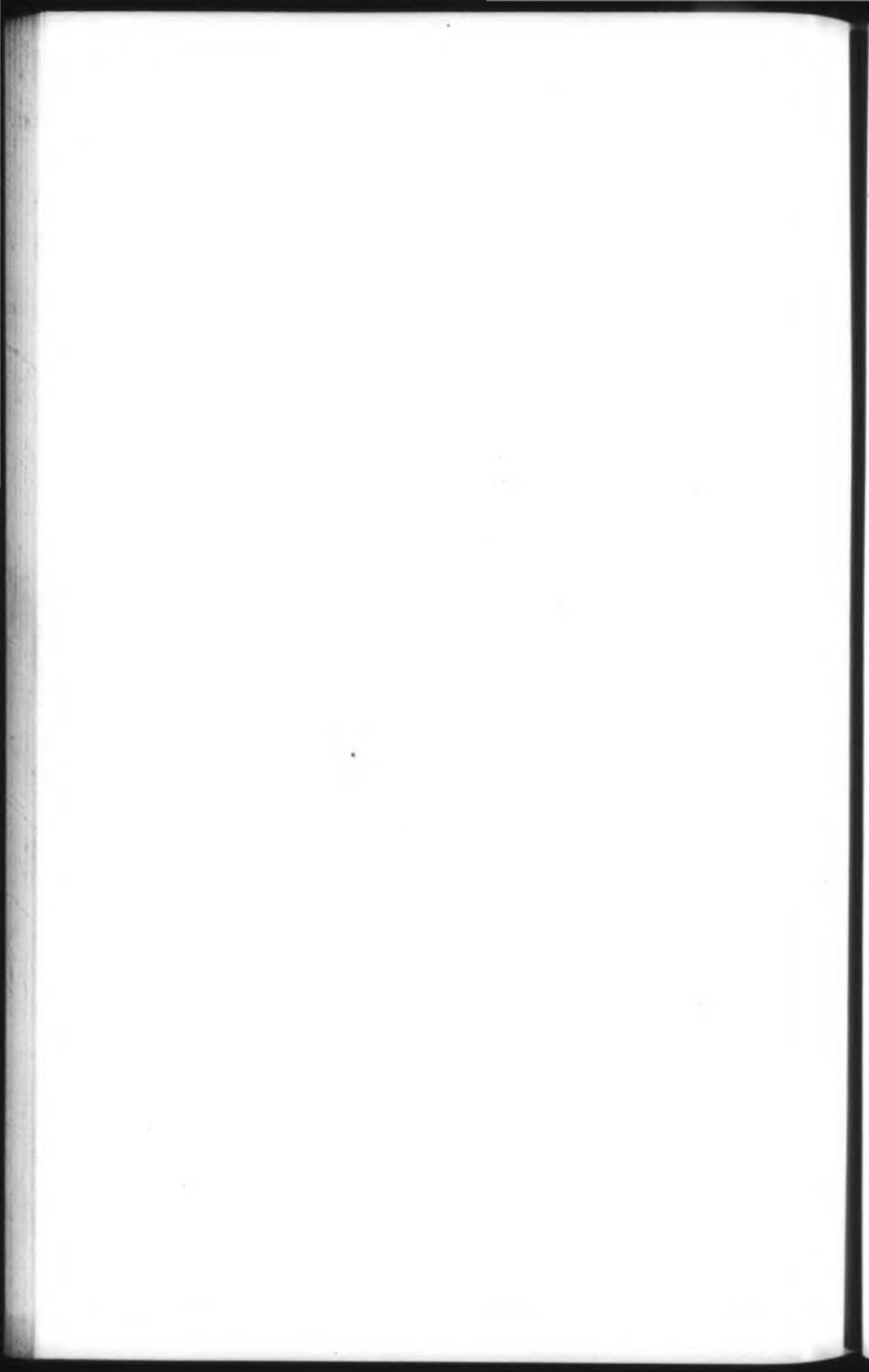
siderable increase in the mileage of roads maintained each year.

In 1923 the cost of maintenance per mile was rather low. This was not due to the type of surface, however, but rather to the lack of maintenance as maintenance of roads at that time in this state was not fully appreciated by the public. In later years a higher type of maintenance has been carried on.

Up to the present our maintenance has covered all types of work and has included not only regular maintenance but reconstruction, and betterment of existing county-built roads that the department has seen fit to take over for maintenance. It is planned to carry on future work and show itemized separation as to regular maintenance, reconstruction and betterment. This will then give due credit to all types and sections of roads.



Typical Section Showing Curb and Gutter Construction.



STATE ROAD DEPARTMENT OF FLORIDA  
SECTIONS OF ROADS UNDER MAINTENANCE 1925.

Road No.	County	Length Miles	From	To	Amount Spent
1	Escambia	5.00	Point 5 Miles West—Pensacola		\$ 1,570.39
1	Santa Rosa	9.00	Pace—Milton		3,379.72
1	Santa Rosa	6.30	Milton—Point 6.3 Miles East		2,295.39
1	Santa Rosa	.80	Blackwater River Bridge and Approaches		1,582.21
1	Santa Rosa	9.60	Project No. 14—Holt		1,246.93
1	Okaloosa	10.50	Holt—Milligan		3,809.41
1	Holmes and Washington	14.00	DeFuniak Springs—Ponce de Leon		6,173.81
1	Holmes and Washington	10.50	Ponce de Leon—Caryville		6,407.65
1	Jackson and Washington	15.00	Chipley—Marianna		3,509.03
1	Jackson	11.15	Marianna—Chipley		1,977.30
1	Jackson	12.00	Cypress—Chattahoochee		3,051.61
1	Jackson and Gadsden	.33	Apalachicola River Bridge		1,915.69
1	Gadsden	5.60	Florence—Quincy		3,221.89
1	Gadsden and Leon	.08	Ocklocknee River Bridge		70.70
1	Leon	17.58	Tallahassee—Jefferson County Line		4,517.17
1	Jefferson	17.25	Leon County Line—Aucilla River		5,821.21
1	Madison	6.00	Aucilla River—Greenville		4,251.92
1	Madison	14.73	Greenville—Madison		8,225.54
1	Madison	15.60	Madison—Suwannee River		23,038.43
1	Suwannee	12.14	Suwannee River—Live Oak		7,851.86
1	Suwannee	13.47	Live Oak—Wellborn		9,073.57
1	Columbia	9.15	Wellborn—Lake City		6,108.56
1	Columbia	9.89	Lake City—Baker County Line		2,181.28

STATE ROAD DEPARTMENT OF FLORIDA  
SECTIONS OF ROADS UNDER MAINTENANCE 1925—(Continued)

Road No.	County	Length Miles	From	To	Amount Spent
1	Baker	15.39	Woodstock—Baldwin		\$ 2,531.49
1	Duval	12.80	Baldwin—Jacksonville		3,778.48
2	Hamilton	12.90	Georgia State Line—Jasper		13,654.29
2	Hamilton	11.86	Jasper—Genoa		7,861.58
2	Hamilton	8.20	Genoa—White Springs		4,356.52
2	Columbia	11.01	White Springs—Lake City		4,715.50
2	Columbia	12.40	Lake City—Project No. 505		3,218.30
2	Columbia	11.10	Project No. 27—Santa Fe River		4,614.70
2	Alachua	11.73	Santa Fe River—Burnett's Lake		1,879.54
2	Alachua	12.60	Burnett's Lake—Gainesville		3,745.63
2	Alachua	14.06	Gainesville—Marion County Line		4,730.59
2	Marion	15.94	Alachua County Line—Ocala		3,593.02
2	Marion	24.68	Ocala—Lake County Line		27,145.39
2	Lake	10.52	Marion County Line—Leesburg		361.88
2	Orange	8.60	Mt. Dora—Plymouth		21,320.45
2	Orange	16.50	Orlando—Osceola County Line		24,042.06
2	Osceola	2.60	Orange County Line—Kissimmee		24,409.21
2	Osceola	8.81	Kissimmee—Polk County Line		10,659.94
2	Hardee	4.52	Bowling Green—Wauchula		4,920.86
2	Hardee	6.00	Wauchula—Peace River		2,251.00
2	Hardee	12.00	Peace River—DeSoto County Line		19,845.93
2	DeSoto	9.00	Hardee County Line—Arcadia		2,885.27
2	DeSoto	6.32	Arcadia—Carlstrom Field		2,385.55
2	DeSoto	7.10	Carlstrom Field—Charlotte County Line		2,408.42

2	Charlotte .....	20.18	DeSoto County Line—Lee County Line....	5,071.96
3	Nassau .....	9.70	Wild's Landing—Yulee .....	5,556.36
3	Clay .....	16.00	Orange Park—Green Cove Springs .....	12,437.07
3	Putnam .....	5.46	Dunn's Creek—Sisco .....	1,402.35
3	Putnam .....	12.80	Sisco—Volusia County Line .....	2,233.20
3	Volusia .....	18.90	Putnam County Line—DeLeon Springs ....	4,039.26
3	Seminole .....	15.50	Sanford—Orange County Line.....	5,353.68
4	Nassau .....	22.50	Georgia State Line—Duval County Line...	9,760.33
4	St. Johns .....	45.00	Duval County Line—Flagler County Line..	21,670.31
4	Flagler .....	13.00	St. Johns County Line—Bunnell .....	3,051.95
4	Flagler .....	10.00	Bunnell—End Reilley's Asphalt .....	3,973.27
4	Volusia .....	7.72	Allendale—New Smyrna .....	56.27
4	Volusia .....	16.74	New Smyrna—Brevard Co. Line .....	306.07
4	Brevard .....	13.60	Volusia County Line—Titusville .....	10,232.93
4	Brevard .....	6.72	Titusville—Project No. 546.....	6,221.87
4	Brevard .....	5.00	Point 5 Miles North—Sharpes .....	4,741.17
4	Brevard .....	15.00	Bonaventure—Melbourne .....	9,341.26
4	Brevard .....	16.17	Melbourne—Sebastian River .....	6,153.42
4	Indian River .....	7.12	Sebastian River—Wabasso .....	7,914.58
4	Indian River .....	7.72	Wabasso—Vero .....	9,020.64
5	Levy .....	20.06	Alachua County Line—Marion County Line.	4,886.09
5	Marion .....	11.30	Levy County Line—Dunnellon .....	3,756.97
5	Citrus .....	10.86	Holder—Inverness .....	171.71
5	Citrus .....	6.61	Inverness—Floral City .....	2,122.72
5	Hernando .....	9.51	Brooksville—Pasco County Line.....	6,639.79
5	Pasco .....	20.08	Hernando Co. Line—Hillsboro Co. Line...	10,343.94
5	Manatee .....	7.07	Hillsboro County Line—Bullard's Creek...	1,242.74
5	Manatee .....	5.00	Bradenton—Sarasota County Line.....	359.11
5	Sarasota .....	5.68	Myakka River—Charlotte County Line....	7,229.95
5	Charlotte .....	10.90	Sarasota County Line—Punta Gorda .....	4,820.98
5	Charlotte .....	9.50	Punta Gorda—Lee County Line.....	15,316.31

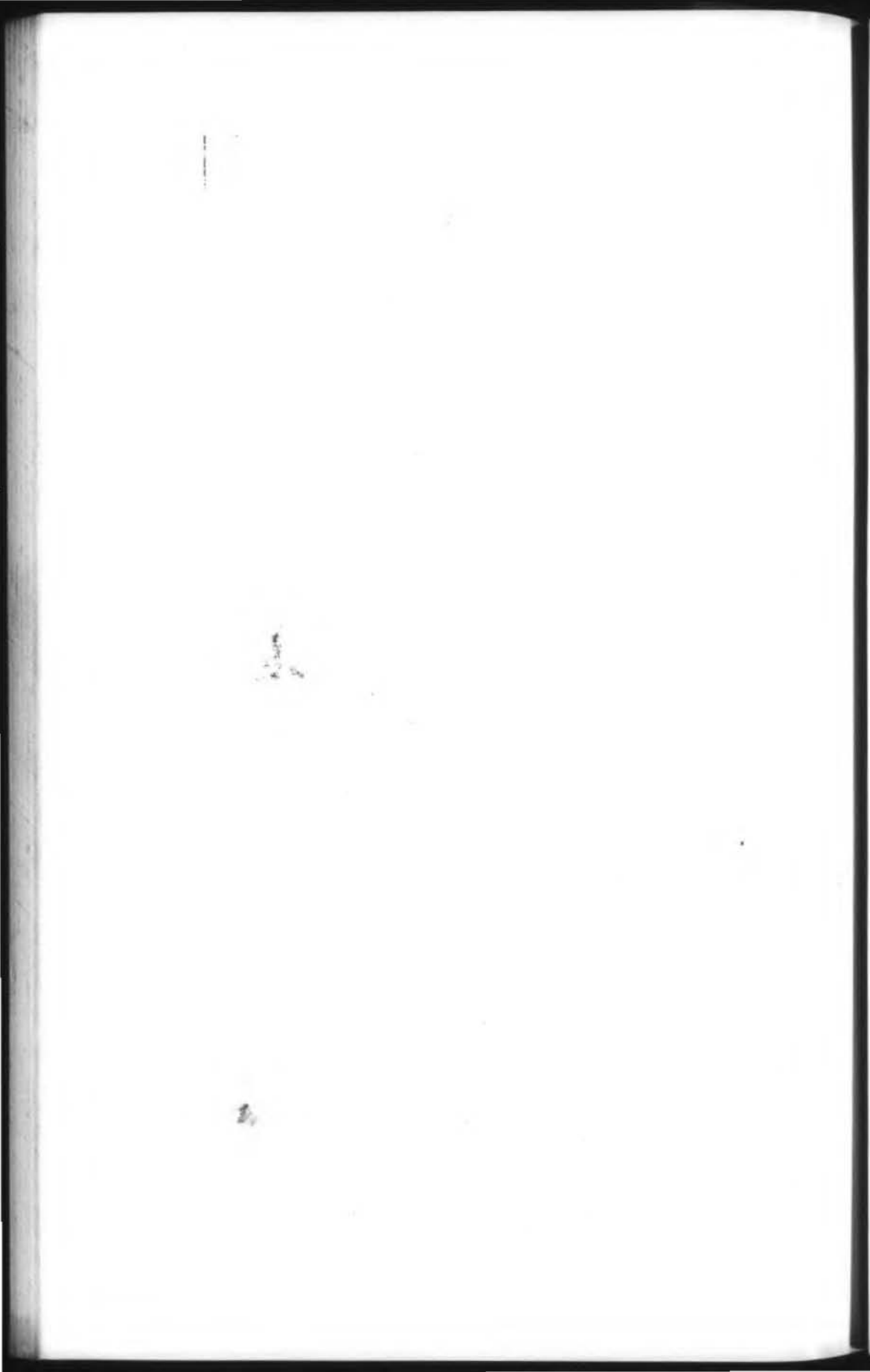
STATE ROAD DEPARTMENT OF FLORIDA  
SECTIONS OF ROADS UNDER MAINTENANCE 1925—(Continued)

Road No.	County	Length Miles	From	To	Amount Spent
6	Calhoun	42.00	Jackson County Line—Wewahitchka		\$ 13,452.62
6	Gulf	26.00	Wewahitchka—Port St. Joe		7,345.27
7	Escambia	10.00	Roberts—Pensacola		2,301.65
8	Polk	21.00	Haines City—Frostproof		54,635.86
8	Polk-Highlands	11.24	Frostproof—Avon Park		3,883.83
8	Highlands	5.37	Josephine Creek—Lake Stearns		32,144.22
8	Highlands	7.00	Project No. 629—Project No. 630		55,309.91
8	Highlands	.57	Kissimmee River Bridge		669.49
8	Okeechobee	8.00	Kissimmee River—Okeechobee		2,304.32
8	Okeechobee	8.00	Okeechobee—St. Lucie County Line		4,964.11
9	Madison	11.66	Georgia State Line—Madison		6,352.24
10	Leon	18.08	Georgia State Line—Tallahassee		250.71
10	Wakulla	5.00	Leon County Line—Wakulla River		2,400.76
10	Wakulla	17.00	Crawfordville—Ocklocknee River		942.28
10	Franklin	18.00	Ocklocknee River—Carrabelle		2,563.66
12	Liberty	25.00	Bristol—Gadsden County Line		3,242.53
13	Clay-Duval	7.00	Maxville—Highlands		3,138.39
13	Bradford	9.10	Starke—Hampton		3,476.67
13	Alachua	16.00	Bradford County Line—Gainesville		3,325.60
17	Polk-Hillsboro	11.00	Lakeland—Plant City		84.00
17	Hillsboro	15.00	Tampa—Oldsmar		120.00
18	DeSoto	3.17	Arcadia—Dorr's Field		1,196.55
20	Jackson	16.00	Cottondale—Bay County Line		6,862.02
20	Bay	35.00	Jackson County Line—Panama City		34,558.10
24	Brevard	15.00	Melbourne—Oceola County Line		4,050.22
Totals		1,270.30			\$ 742,370.20





Sheet Asphalt on Lime Rock Base Road No. 4, Dade County.



STATE ROAD DEPARTMENT OF FLORIDA  
SECTIONS OF ROADS UNDER MAINTENANCE 1926.

Road No.	County	Length Miles	From	To	Amount Spent
1	Escambia	15.60	Perdido River—Pensacola		\$ 9,594.11
1	Escambia & Santa Rosa	3.60	Escambia Bay Bridge and Approaches		38,516.06
1	Santa Rosa	16.40	Escambia Bay Bridge—Project No. 590		12,968.30
1	Santa Rosa	0.80	Blackwater River Bridge and Approaches		1,729.62
1	Santa Rosa	20.00	Brick Pavement—Milligan		6,144.00
1	Okaloosa & Walton	26.40	Delaco—Argyle		14,514.73
1	Holmes & Walton	18.60	Argyle—Caryville		18,061.08
1	Washington & Holmes	8.50	Bonifay—Chipley		162.62
1	Jackson	17.40	Chipley—Marianna		26,722.39
1	Jackson	11.15	Marianna—Cypress		8,380.34
1	Jackson	12.00	Cypress—Chattahoochee		2,292.01
1	Jackson & Gadsden	1.00	Victory & Pope Lake Bridges		2,072.06
1	Gadsden	20.20	Chattahoochee—Quincy		10,222.36
1	Leon	7.00	Gadsden County Line—Tallahassee		2,748.76
1	Leon	17.58	Tallahassee—Jefferson County Line		17,326.89
1	Jefferson	17.28	Leon County Line—Aucilla River		17,822.85
1	Madison	6.00	Aucilla River—Greenville		4,164.79
1	Madison	14.73	Greenville—Madison		16,477.20
1	Madison	15.60	Madison—Suwannee River		16,037.81
1	Suwannee	12.14	Suwannee River—Live Oak		14,459.19
1	Suwannee	13.47	Live Oak—Wellborn		10,604.63
1	Columbia	9.15	Wellborn—Lake City		11,166.85

STATE ROAD DEPARTMENT OF FLORIDA  
SECTIONS OF ROADS UNDER MAINTENANCE 1926—(Continued)

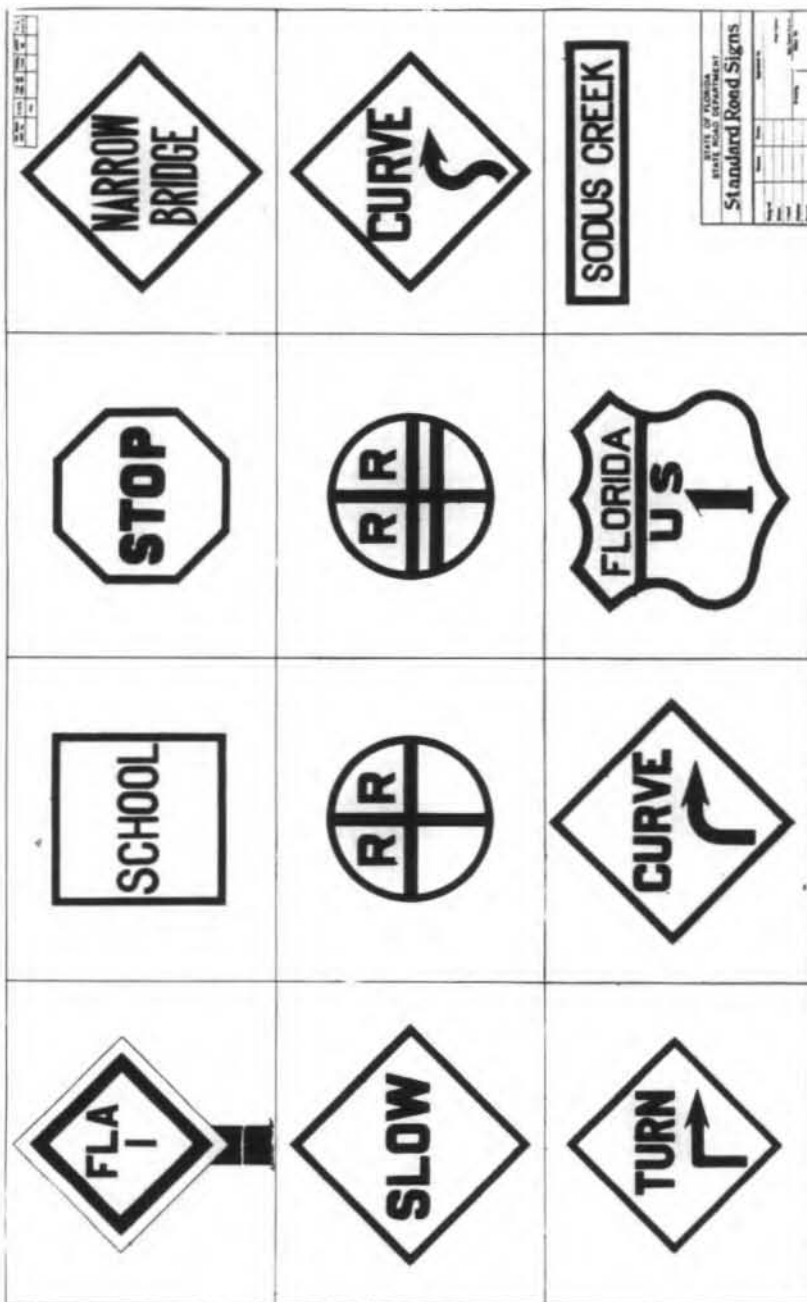
Road No.	County	Length Miles	From	To	Amount Spent
1	Columbia	9.89	Lake City—Baker County Line		\$ 7,932.95
1	Baker	15.39	Woodstock—Baldwin		4,906.40
1	Duval	12.80	Baldwin—Jacksonville		5,709.87
2	Hamilton	12.90	Georgia State Line—Jasper		14,269.12
2	Hamilton	11.86	Jasper—Genoa		32,212.73
2	Hamilton	8.20	Genoa—Suwannee River		16,182.76
2	Columbia	11.01	Suwannee River—Lake City		9,435.12
2	Columbia	23.50	Lake City—Santa Fe River		9,583.22
2	Alachua	24.33	Columbia County Line—Gainesville		11,477.30
2	Alachua	14.06	Gainesville—Marion County Line		6,675.78
2	Marion	10.92	Alachua County Line—Lowell		4,820.68
2	Marion	10.44	Lowell—Ocala		4,222.01
2	Marion	9.15	Ocala—Bellevue		4,521.09
2	Marion	15.53	Bellevue—Lake County Line		3,669.34
2	Lake	10.52	Marion County Line—Leesburg		4,874.99
2	Lake	3.52	Leesburg—Helena Run		606.97
2	Orange	8.60	Mt. Dora—Plymouth		6,186.50
2	Orange	16.00	Plymouth—Orlando		69,740.50
2	Orange	16.50	Orlando—Osceola County Line		64,955.95
2	Osceola	2.60	Orange County Line—Kissimmee		4,296.84
2	Osceola	8.81	Kissimmee—Polk County Line		13,823.12
2	Hardee	4.52	Bowling Green—Wauchula		4,076.83
2	Hardee	6.00	Wauchula—Peace River		29,038.96

2	Hardee	12.00	Peace River—DeSoto County Line	53,146.59
2	DeSoto	9.00	Hardee County Line—Arcadia	7,459.23
2	DeSoto	6.32	Arcadia—Carlstrom Field	44,080.29
2	DeSoto	7.10	Carlstrom Field—Charlotte County Line	8,462.25
2	Charlotte	20.18	DeSoto County Line—Lee County Line	5,303.28
3	Nassau	9.70	Wild's Landing—Yulee	5,769.74
3	Nassau	11.52	St. Mary's River—Yulee	4,395.20
3	Clay	16.00	Orange Park—Green Cove Springs	8,206.14
3	Putnam	10.82	Clay County Line—Rice Creek	3,793.83
3	Putnam	11.73	San Mateo—Sisco	3,419.94
3	Putnam	12.80	Sisco—Volusia County Line	3,010.60
3	Putnam	2.34	Through the town of Crescent City	1,002.04
3	Volusia	18.90	Putnam County Line—DeLeon Springs	2,843.28
3	Volusia	7.43	DeLeon Springs—Deland	774.88
3	Volusia	12.00	Deland—Monroe Bridge	11,905.87
3	Volusia	.28	Monroe Bridge	12,730.20
3	Seminole	14.20	Lake Monroe—Maitland	91,691.34
4	Nassau	22.50	Georgia State Line—Duval County Line	11,352.05
4	St. Johns	45.00	Duval County Line—Flagler County Line	268,684.03
4	Flagler	13.00	St. Johns County Line—Bunnell	68,605.74
4	Flagler	10.00	Bunnell—End Reilley's Asphalt	4,973.57
4	Volusia	7.72	Allendale—New Smyrna	7,841.34
4	Volusia	16.74	New Smyrna—Brevard County Line	8,340.93
4	Brevard	13.60	Volusia County Line—Titusville	16,606.28
4	Brevard	6.72	Titusville—Project No. 546	7,663.01
4	Brevard	5.00	Point 5 Miles North—Sharpes	5,322.80
4	Brevard	15.00	Bonaventure—Melbourne	17,417.39
4	Brevard	16.17	Melbourne—Sebastian River	9,453.72
4	Indian River	7.12	Sebastian River—Wabasso	4,557.93
4	Indian River	7.72	Wabasso—Vero	6,076.22
5-A	Alachua	1.67	Santa Fe River—High Springs	72.85

STATE ROAD DEPARTMENT OF FLORIDA  
SECTIONS OF ROADS UNDER MAINTENANCE 1926—(Continued)

Road No.	County	Length Miles	From	To	Amount Spent
5	Alachua	17.06	High Springs—Newberry Archer Line....	\$	461.65
5	Levy	20.06	Alachua County Line—Marion County Line		5,983.59
5	Marion	11.30	Levy County Line—Dunnellon		4,577.27
5	Citrus	10.86	Holder—Inverness		4,348.86
5	Citrus	6.61	Inverness—Floral City		4,018.68
5	Citrus & Hernando	15.14	Floral City—Brooksville		1,292.76
5	Hernando	9.51	Brooksville—Pasco County Line		7,928.99
5	Pasco	20.08	Hernando County Line—Hillsboro Co. Line		12,606.22
5	Hillsboro	12.30	Six Mile Creek—Point 12.3 Miles South....		6,879.13
5	Hillsboro	11.20	Project No. 581—Manatee County Line		1,158.71
5	Manatee	5.00	Bradenton—Sarasota County Line		4,029.47
5	Sarasota	4.62	Manatee County Line—Sarasota		1,913.59
5	Sarasota	5.68	Myakka River—Charlotte County Line		4,005.15
5	Charlotte	10.90	Sarasota County Line—Punta Gorda		7,201.37
5	Charlotte	9.50	Punta Gorda—Lee County Line.....		18,255.11
5	Lee	8.27	Charlotte Co. Line—Caloosahatchee River		1,896.87
6	Jackson	20.00	Alabama State Line—Marianna		5,308.00
6	Jackson	5.05	Marianna—South		3,795.58
6	Calhoun	42.00	Jackson County Line—West Arm Bridge		21,700.39
6	Gulf	14.00	West Arm Bridge—Cypress Creek		4,843.00
6	Gulf	12.00	Cypress Creek—Port St. Joe		5,744.97
7	Escambia	17.00	Alabama State Line—Pine Barren Hill		7,343.77
7	Escambia	6.30	Pine Barren Hill—Beginning of Pavement..		578.40

7	Escambia	18.70	Project No. 51—Pensacola	8,045.69
8	Polk	21.00	Haines City—Frostproof	181,392.99
8	Polk & Highlands	11.24	Frostproof—Avon Park	4,461.46
8	Highlands	5.37	Josephine Creek—Lake Stearns	2,999.89
8	Highlands	24.00	Lake Annie—Kissimmee River	10,776.86
8	Highlands & Okeechobee	.57	Kissimmee River Bridge	1,019.44
8	Okeechobee	8.00	Kissimmee River—Okeechobee	5,834.82
8	Okeechobee	8.00	Okeechobee—St. Lucie County Line	2,434.03
8	St. Lucie	17.30	Okeechobee County Line—East	8,429.29
9	Madison	11.66	Georgia State Line—Madison	5,789.33
10	Leon	18.00	Georgia State Line—Tallahassee	10,564.80
10	Wakulla	5.50	Leon County Line—Wakulla River	4,642.89
10	Wakulla	18.00	Crawfordville—Ochlockonee River	6,065.05
10	Franklin	18.00	Ochlockonee River—Carrabelle	4,403.11
12	Liberty	25.00	Bristol—Gadsden County Line	4,156.75
13	Duval	7.50	Baldwin—Maxville	17,633.81
13	Clay	7.00	Maxville—Highlands	11,050.26
13	Bradford	5.10	Highlands—Lawtey	1,216.24
13	Bradford	7.50	Lawtey—Starke	5,160.75
13	Bradford	9.10	Starke—Hampton	4,579.97
13	Alachua	16.00	Bradford County Line—Gainesville	10,386.87
14	St. Johns & Putnam	7.00	Hastings—East Palatka	42,286.97
15	Pinellas	22.50	St. Petersburg—Clearwater	1,779.27
15	Pinellas	15.00	Clearwater—Tarpon Springs	2,642.53
17	Polk	11.00	Lakeland—Plant City	80.00
17	Hillsboro	15.00	Tampa—Oldsmar	120.00
17	Pinellas	12.00	Hillsboro County Line—Clearwater	1,519.89
18	DeSoto	3.16	Arcadia—Dorr's Field	22,040.21
20	Jackson	16.60	Cottontdale—Bay County Line	9,467.53
20	Bay	35.00	Jackson County Line—Panama City	12,075.06
24	Brevard	15.00	Melbourne—Osceola County Line	8,651.41
33	Okaloosa	19.47	Alabama State Line—Crestview	10,085.27
Totals		1,651.21		\$1,858,034.21





## HIGHWAY MARKINGS

Some two years ago the American Association of State Highway Officials in conjunction with the United States Bureau of Public Roads, realized the importance of standardizing the highway markers, and at a meeting of the Association of State Highway Officials a Joint Advisory Board was appointed, composed of members of the Association and the Bureau of Public Roads, acting as directing head, to investigate and make an exhaustive study of highway markers and signs.

This Board immediately started functioning and in connection with their investigation and duties they held meetings at various central points over the United States and invited all interested parties to attend. These meetings were in the nature of general discussions with a view to exchanging ideas on the various types of signs. After holding meetings all over the country the Joint Advisory Board then compiled all of the data obtained and drew up a tentative design for road markers and signs as a standard; this was then in turn submitted back to the American Association of State Highway Officials for consideration and further questions and arguments were invited. After carrying this on for a period of two years a standard for road markers and signs was adopted by the Association and that is the standard which we are now following in the erection of signs and markers on our highways.

The system of standardized signs and markers as adopted by the American Association of State Highway Officials at Detroit in November, 1925, is developed in a set of working drawings prepared by the Bureau of Public Roads and adopted by the Association. This set of designs is based on definite principles calculated to produce uniformity of significance in the signs themselves and make familiarity with them easy to acquire on the part of the most casual driver. These principles are a set of shapes, each having its own

significance; a set of color combinations, each having its own significance; a few obvious symbols; and uniformity of erection and application as described in their manual.

A circular sign is used for railroad grade crossings only.

Octagonal signs are used to indicate "Stop" where for any reason such action is necessary.

A diamond shape sign is used to indicate any condition inherent in the road itself requiring slow speed and caution on the part of the driver.

A square sign is used to indicate any condition requiring caution that is not inherent in the road itself, but which is due to contiguous or adjacent conditions which often are also intermittent.

Rectangular signs of various dimensions are used to carry directions and information of use or benefit to the driver.

All signs of a precautionary character, including the circular railroad signs, octagonal stop signs, the diamond caution signs and square caution signs, have black letters and designs on a yellow background.

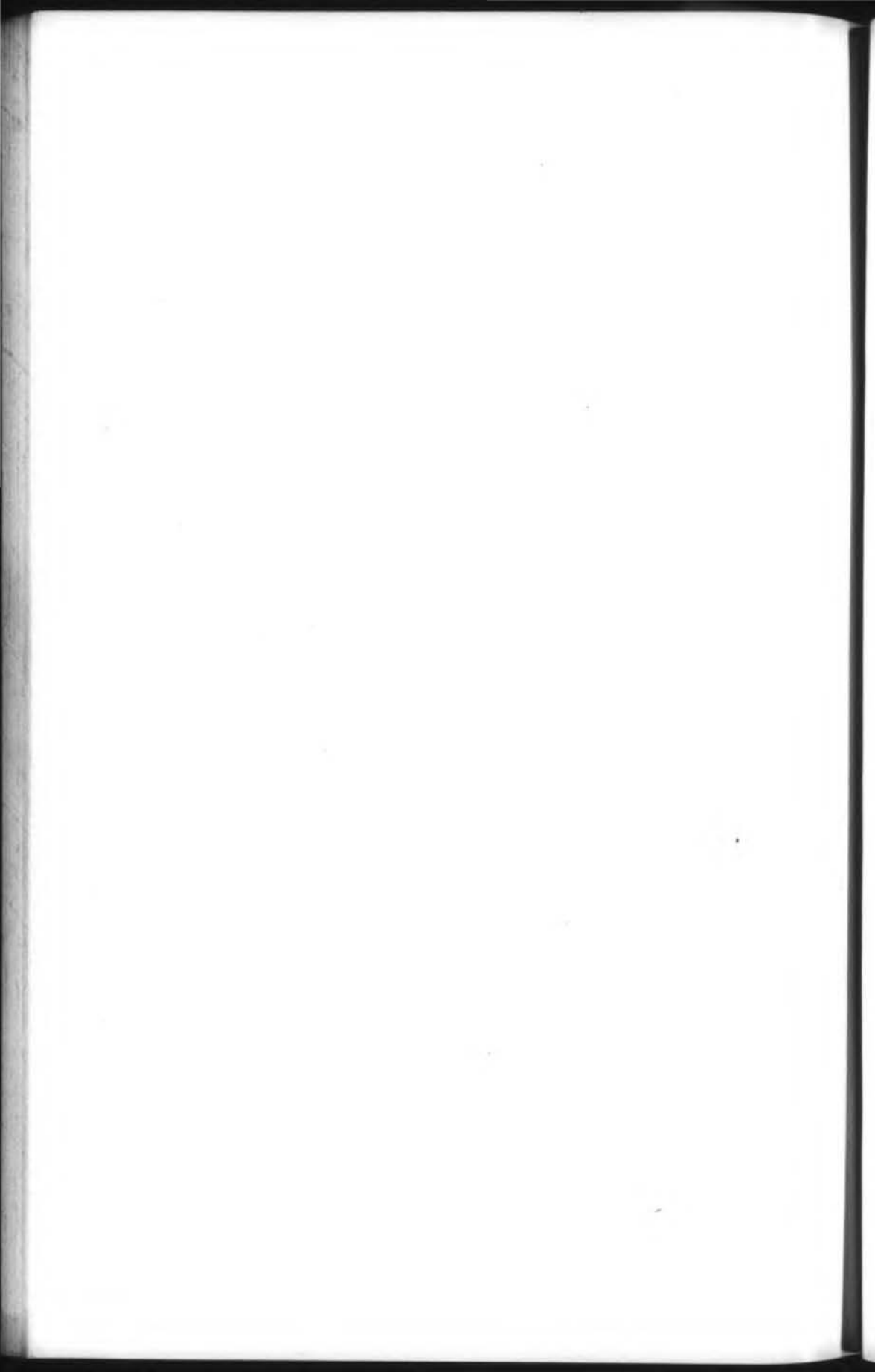
All directional signs are black on a white background.

The symbols used are those for railroad grade crossings in single and multiple tracks; for left and right curves and turns and for reverse curves and turns.

The above is the standard as adopted by the Association and the standard route marker for U. S. Highways is an outline of the official shield of the United States, but each individual State may use whatever appropriate marker they desire to adopt as their standard and place it in conjunction with all other signs. For instance, the official route marker for Nebraska is the Covered Wagon; the Indian Head for North Dakota, and the Keystone in Pennsylvania. This Department adopted as its official route marker, that is the symbol carrying the road number, a twelve-inch diamond shape sign with the letters FLA and the road number. The letters and numbers are white on a



Florida Rock Base. Road No. 1. Columbia County.



blue background, same making a very pleasing and appropriate marker.

Prior to the erection of our signs a complete log was made of the roads that we contemplated marking at this time. This log was made by one person in the same car and was made for the purpose of securing data and showing points of erection for each and every sign, the idea being uniformity throughout, as well as securing accurate distances. All distances are speedometer measurements corrected for the ratio of error of the speedometer, same having been checked over two hundred miles of measured distance. Our erection crews are following the schedule as outlined from this log of roads.

In addition to the above mentioned warning signs directional signs have been placed at all points wherever needed for the guidance of the traveler, both on the highways and, especially, routes through the towns and cities. In carrying out the directional program the policy adopted was to consider the road, giving the distance to the nearest town at its termini and the nearest local town in either direction.

The roads logged and marked are as follows: Numbers 1, 2, 3, 4, 5, 6, 7, 8, 9, 10 from the Georgia line to Tallahassee, 14 from Newberry to Hastings, 15 from Clearwater to St. Petersburg, 17, 21, 22 from Orlando to Indian River City, 23 and 24, these being the heaviest traveled roads at present and in a more continuous state of pavement than any of the others. It is the plan of the Department to continue to add to the above list of roads others until ultimately we have marked our entire system in accordance with the standard as outlined above and adopted.

## ACCOUNTING DEPARTMENT

S. L. Walters, Auditor

## ACCOUNTING METHODS

Since last report, our Receipts and Disbursements have increased from \$600,000.00 to \$2,000,000.00 monthly.

Balance Brought Forward—January 1st, 1925.....\$ 547,323.65  
Receipts for the year 1925:

5% Auto-Administration Fund.....\$	164,768.30	
70% Auto-State Federal Aid.....	2,306,727.50	
State Road Tax .....	505,109.94	
From Federal Government.....	1,080,574.24	
Gasoline Inspection Fund .....	169,308.01	
Gasoline—3c Gallon Tax Fund....	4,355,634.61	
Auto Registration Fund .....	80,000.00	
From Counties .....	1,689,856.43	
Miscellaneous .....	32,886.03	10,384,865.06

Receipts for the Year 1926:

5% Auto-Administration Fund ....	314,244.20	
70% Auto-State Federal Aid .....	4,399,418.71	
State Road Tax .....	524,426.09	
From Federal Government .....	1,007,484.16	
Gasoline Inspection Fund .....	210,842.46	
Gasoline—3c Gallon Tax Fund....	8,636,358.93	
Auto Registration Fund .....	200,000.00	
From Counties .....	614,137.61	
Miscellaneous .....	38,588.56	15,945,500.72

Total .....\$26,877,689.43

## DISBURSEMENTS

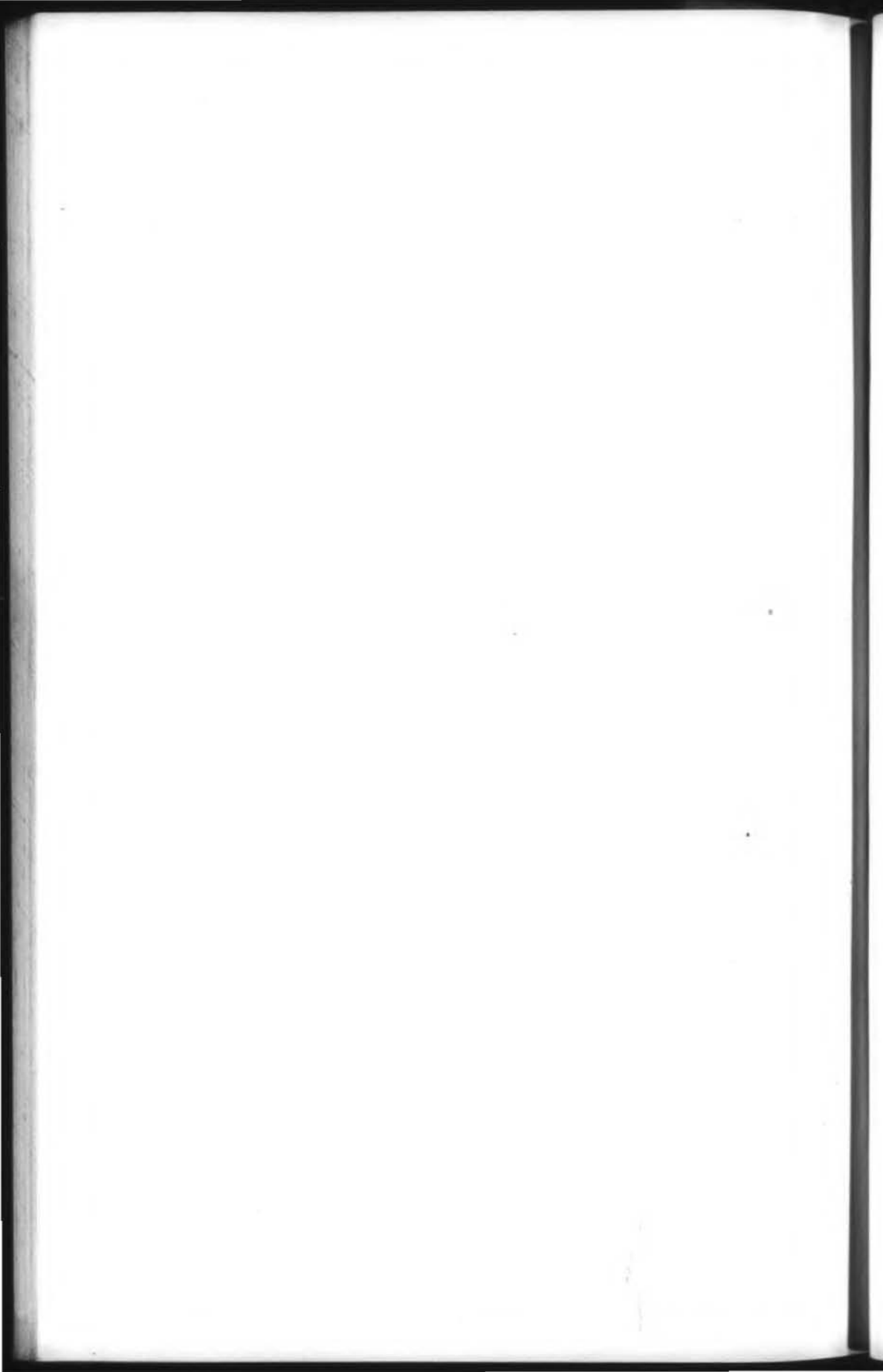
Administration and General Office Expense .....	\$ 102,617.02
Cost of Maintaining Roads and Bridges .....	2,646,146.32
Cost of Constructing Roads and Bridges .....	18,767,632.04
Maintaining State Convicts .....	612,770.31

Total .....\$22,129,165.69  
Balance to January 1st, 1927 .....\$ 4,748,523.74

Believing a summary of Receipts, since organization of Department, will be of interest, have prepared following tables:



Bituminous Macadam Road No. 4, Indian River County.





STATE ROAD DEPARTMENT OF FLORIDA  
RECEIPTS FROM ALL SOURCES  
NOVEMBER, 1915, TO DECEMBER 31st, 1926

Year	Auto License	Ad Valorem Tax	Federal Government	Gasoline	County Donations	Miscellaneous	Total
1915 .....	\$ 3,646.90	.....	.....	.....	.....	.....	\$ 3,646.90
1916 .....	30,246.23	.....	.....	.....	.....	.....	30,246.23
1917 .....	21,439.42	.....	.....	.....	\$ 238.79	.....	21,678.21
1918 .....	278,464.42	\$ 175,842.11	\$ 9,972.55	.....	11,111.22	.....	475,390.30
1919 .....	349,478.45	290,532.41	11,759.19	.....	71,157.76	\$ 1,612.68	724,540.49
1920 .....	499,500.00	694,814.77	209,060.84	\$ 17,850.32	239,320.49	3,575.77	1,664,122.19
1921 .....	671,534.91	643,373.16	490,308.38	283,881.15	111,708.76	3,322.77	2,204,129.13
1922 .....	1,074,352.71	396,206.19	842,965.84	682,139.85	580,716.92	9,247.27	3,585,628.78
1923 .....	1,394,528.58	427,243.17	820,063.74	1,353,729.78	515,988.35	132,586.29	4,644,139.91
1924 .....	1,758,699.27	424,392.88	1,071,287.04	2,575,181.15	879,219.83	75,881.79	6,784,661.96
1925 .....	2,551,495.80	505,109.94	1,080,574.24	4,524,942.62	1,689,856.43	32,886.03	10,384,865.06
1926 .....	4,913,662.91	524,426.09	1,007,484.16	8,847,201.39	614,137.61	38,588.56	15,945,500.72
Total .....	\$13,547,049.60	\$ 4,081,940.72	\$ 5,543,475.98	\$18,284,926.26	\$ 4,713,456.16	\$ 297,701.16	\$46,468,549.88

STATE ROAD DEPARTMENT OF FLORIDA  
DISBURSEMENTS

NOVEMBER, 1915, TO DECEMBER 31st, 1926

Year	Auto License	Ad Valorem Tax	Federal Government	Gasoline	County Donations	Miscellane- ous	Total	Balances
1915 ..	\$ 974.26	.....	.....	.....	.....	.....	\$ 974.26	\$ 2,672.64
1916 ..	13,375.41	.....	.....	.....	.....	.....	13,375.41	19,543.46
1917 ..	23,952.39	.....	.....	.....	\$ 238.79	.....	24,191.18	17,030.49
1918 ..	140,907.02	\$ 49,324.16	.....	.....	11,111.22	.....	201,342.40	291,078.39
1919 ..	320,262.71	287,349.91	\$ 21,105.63	.....	71,157.76	.....	699,876.01	315,742.87
1920 ..	284,710.18	781,460.11	205,659.06	\$ 17,846.28	239,320.49	.....	1,528,996.12	450,868.94
1921 ..	722,572.09	616,014.12	431,995.21	220,987.51	111,708.76	.....	2,103,277.69	551,720.38
1922 ..	1,292,896.63	373,629.70	720,896.84	415,933.65	580,716.92	.....	3,384,073.74	753,275.42
1923 ..	1,425,503.76	437,257.98	931,637.36	1,312,595.13	515,988.35	\$ 62,636.89	4,685,619.47	711,795.86
1924 ..	1,835,563.18	426,696.69	1,141,589.10	2,586,708.56	879,219.83	79,357.31	6,949,134.67	547,323.15
1925 ..	2,381,319.01	470,844.43	1,004,521.80	4,169,498.09	457,568.54	21,285.70	8,505,037.57	2,427,150.64
1926 ..	4,103,061.01	403,712.62	950,698.86	7,166,190.51	1,060,407.26	11,522.45	13,695,592.71	4,677,058.65
Total	\$12,545,097.65	\$ 3,846,289.72	\$ 5,408,103.86	\$15,889,759.73	\$ 3,927,437.92	\$ 174,802.35	\$41,791,491.23	



Road 5, Lee County—Florida Lime-Rock Base.

It is to be understood that receipts shown in preceding tables are those allotted the State Road Department. To illustrate: Gross receipts from sale of Auto License are collected and pooled in Comptroller's office. From this pool, deduction is made to cover cost of tags, postage for mailing, and clerical help in recording. The balance is then divided: 25 per cent remitted direct to counties, and 75 per cent credited to State Road Department. Receipts from Gasoline Tax are handled in similar manner, except that net pool is divided: 1c remitted to counties and 3c credited this department.

### PURCHASES

Groceries for convict camps are purchased on competitive bids. When bids have been tabulated, and formal orders written, they are signed by the Chairman.

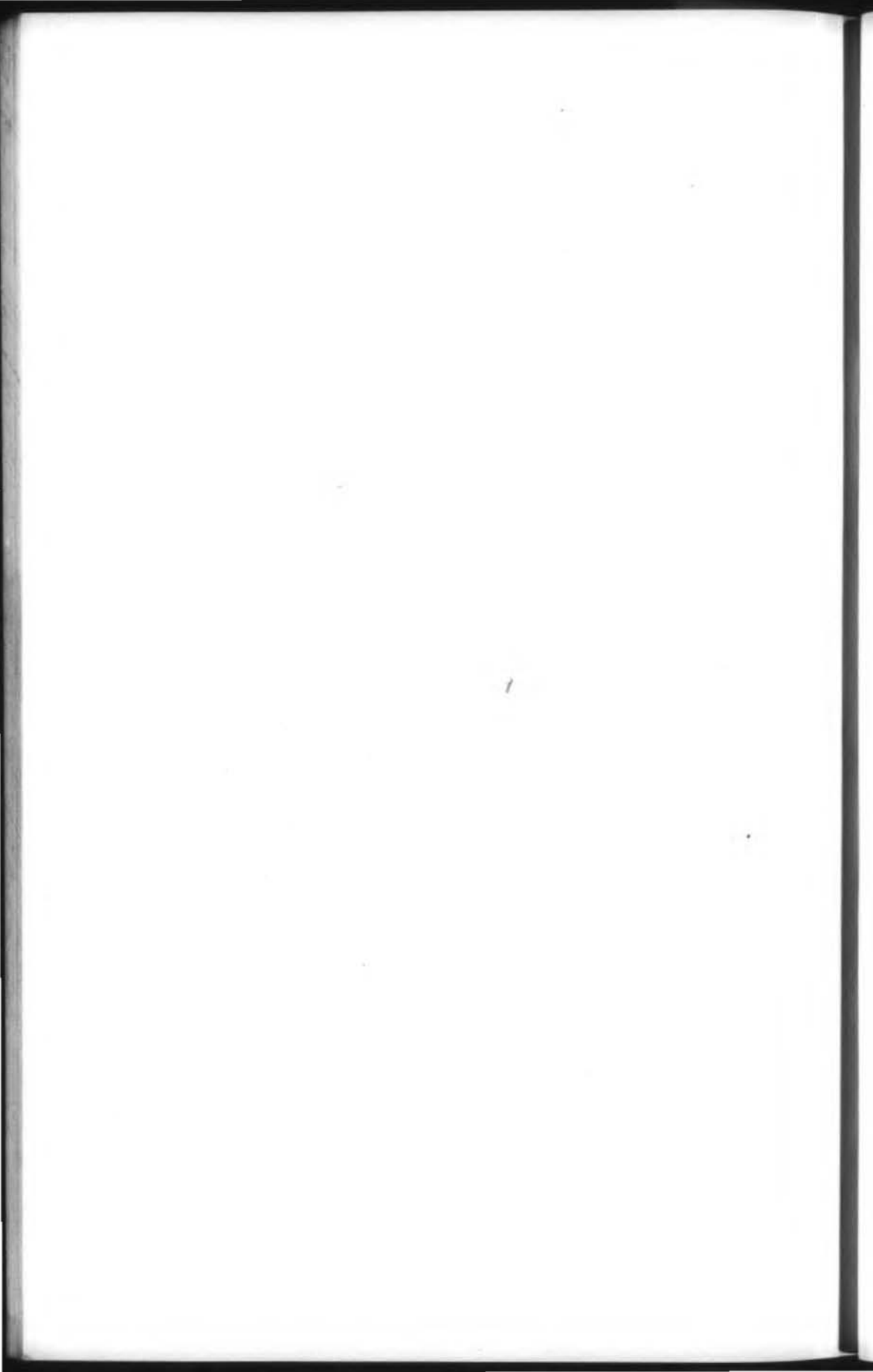
On authority of Chairman, we are authorized to purchase and carry such stationery and small supplies as are needed. These are issued to the various departments on properly approved requisitions. Such printing as can be handled by Printing Department of the Industrial School for Boys, at Marianna, is placed with them.

A statistical table covering cost of maintaining Convict Camps is submitted:

	1925	1926
Groceries .....	\$151,676.36	\$142,306.89
Salaries .....	86,138.31	100,680.97
All Other Expense .....	64,826.01	67,141.77
Daily Average Prisoners in Camps ....	781.3	800.8
Calendar Prisoner Days .....	285,188	292,275
Captain and Guard Days—Calendar ....	42,970	44,086
Total meals served .....	1,011,337	1,033,561
Average Cost per Meal .....	.15	.14
Daily Overhead per Prisoner .....	.53	.57
Daily Cost per Prisoner .....	1.06	1.06



Beautiful Scene On Road 2, Approaching County Bridge Over Santa Fe River.



## GENERAL ACCOUNTING METHODS

Briefly stated, our accounting methods are: No accounts are passed for payment unless approved by proper official of the Department. Miscellaneous accounts, for the month, are accumulated in the Division Offices, and listed to this office on 8th of each month. When bills are received, they are given a careful audit. When audited, they are listed on accumulative vouchers and submitted to State Highway Engineer and Chairman for their approval. When approved, vouchers are transmitted to office of State Comptroller where bills are again audited, and, if found correct, warrants are written. When warrants have been written, they are called for and mailed from this office. Before mailing, we record warrant number, date mailed, and attach to each warrant a memo setting forth accounts being paid. Our system of handling accounts reduces duplication to a minimum and enables prompt handling of correspondence relating thereto.

Monthly reports are prepared for information of Chairman and members of the Board. Such reports set forth detailed data as to Receipts and Disbursements, also other information which we think will be of value.

Only a small percent of our bills are subject to discount. We, however, take advantage of discounts when allowed, and in several instances have arranged discount periods on accounts where formerly discounts were not allowed.

Our accounts have just been audited by Mr. W. E. Murrow, State Auditor, and from his report we quote: "The Accounting of the State Road Department is handled under the supervision of Mr. S. L. Walters, Auditor for the Department, and the records are kept in a very neat and comprehensive manner."

## LEGAL DEPARTMENT

The tremendous increase in the operations of the State Road Department has been reflected in the legal department of this organization, as well as in the other lines of activity. Perplexing legal problems have presented themselves, and have been solved, for the most part, by the manifest desire of all parties to iron out difficulties and to establish precedents without litigation.

Much of the work of the legal department has been performed in connection with matters of rights of way for State roads. At the beginning of the present two-year period, there was an all-too evident inclination on the part of many of the public to deal closely with the State and Counties in the matter of rights of way for State roads. This disposition has been in great measure ameliorated, and today difficulty in securing rights of way is the exception rather than the rule. The great majority of our citizens evidently have realized that although a State road does deprive them of property rights in a part of their holdings, the net result in the great majority of cases is to enhance the value of the remaining lands many times the value of the lands taken.

The Department has offered the cooperation of its attorney to the counties in securing these rights of way, and cases have been tried with the assistance of the Department's attorney in many Florida counties. Besides the cases tried, preparatory assistance has been rendered in every case where application was made. The satisfactory results which have been obtained have been due in no small part to the intelligent and ready cooperation of the Boards of County Commissioners of the several counties, and of their attorneys.

Litigation, fortunately, has been at a minimum. The most important case tried during this biennium was one in which the constitutionality of Section 2 of Chapter 10,118, Laws of 1925, was attacked. After exhaustive





Looking North at Palm Bay, F. A. Proj. 40-A—Road 4—Surface Treated Rock Base

briefs submitted on each side, the Supreme Court held, last August, that the section in question is repugnant to the Constitution and as a result the same is now inoperative. This was the section which permitted the Department to enter upon lands of a property owner and commence work pending condemnation when double the value of the property taken, as fixed by the Circuit Judge on affidavits of three disinterested freeholders owning lands in the vicinity, was paid into registry of the Court.

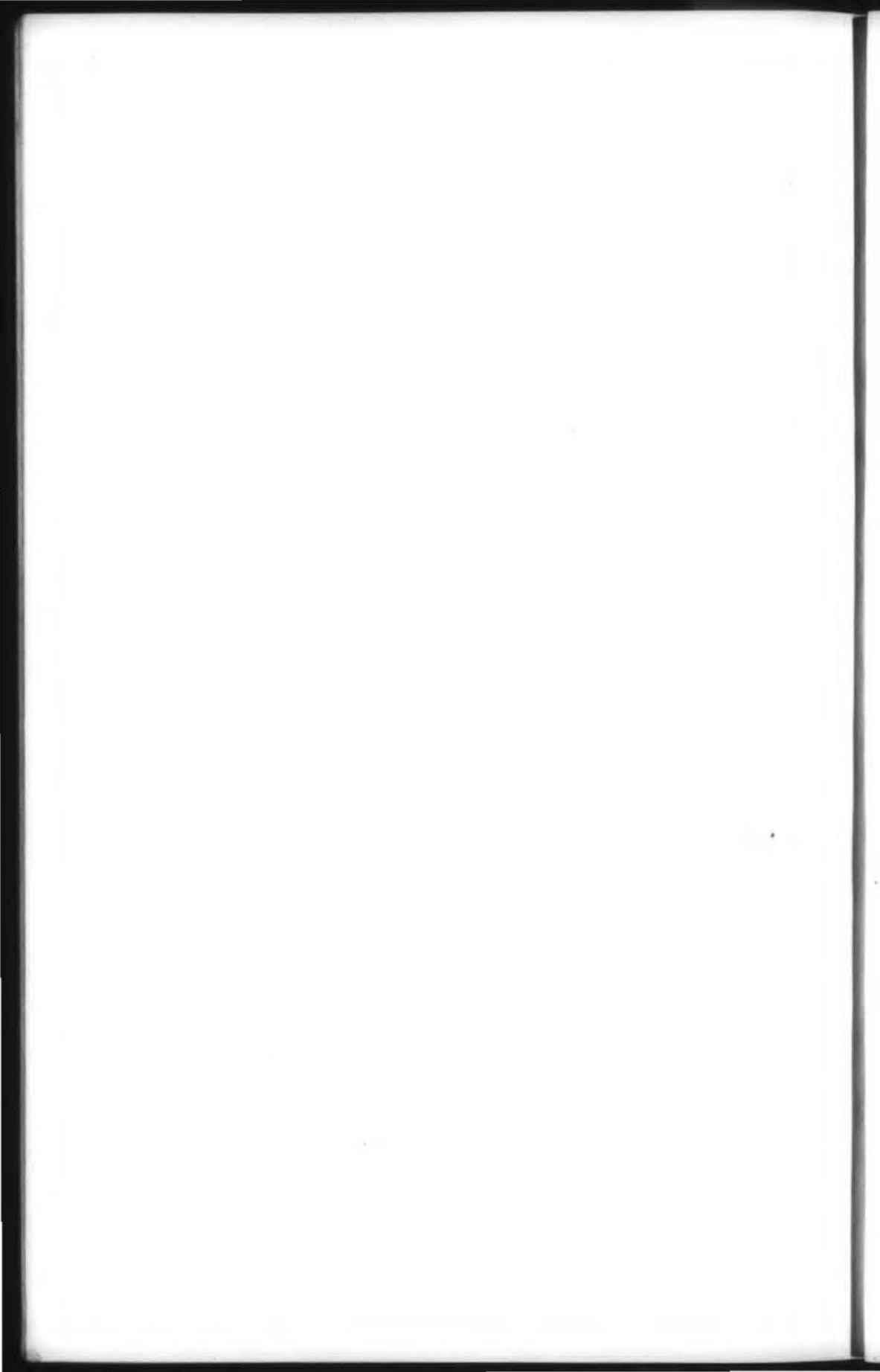
At the present time there are pending against the Department but three causes of action; two growing out of contract, and one in tort. The first two rest upon claims of non-payment under circumstances which the Department conceives to render it without liability, and the action in tort grows out of an alleged trespass upon lands in a case where the Department was assured by the county that the necessary right of way had been secured; and where the new line had been established by the county under the posting statute. It is probable that the latter will have been tried when this report appears.

It was not to be expected that an expenditure of eighteen millions of dollars per year could be made without misunderstanding at some points, nor entirely without litigation. The minimum of litigation is a credit to the manner in which the Department transacts business, since it can now be sued on claims arising out of contracts for work done.

Detail work of the legal department such as examination of claims, taking and preparation of deeds, preparation of contracts, and advice to those seeking it form no inconsiderable part of the necessary work and require constant and unremitting attention.

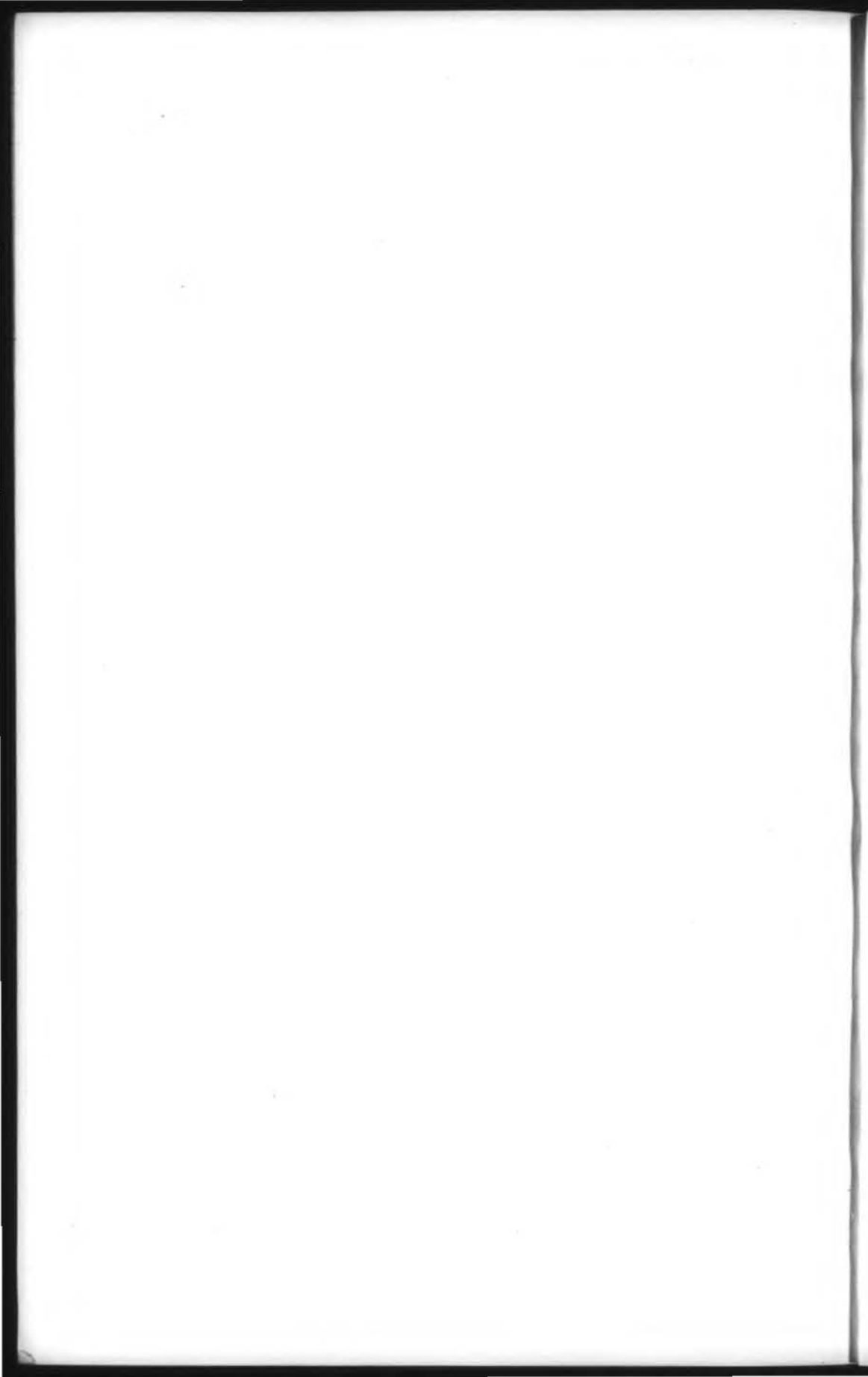


Drilling, First Operation in Construction of Tamiami Trail.



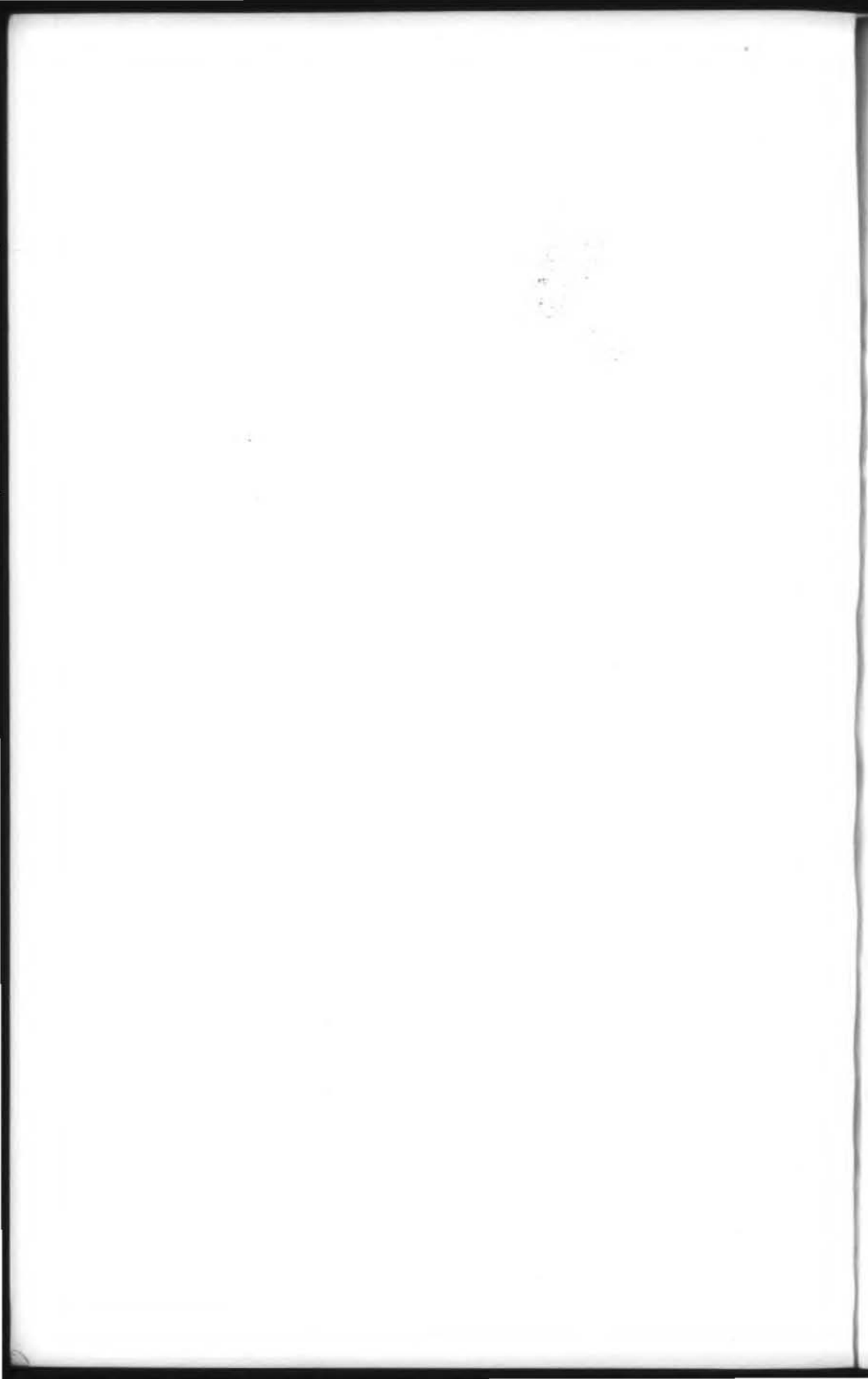


Blasting, Second Operation in Building Tamiami Trail.





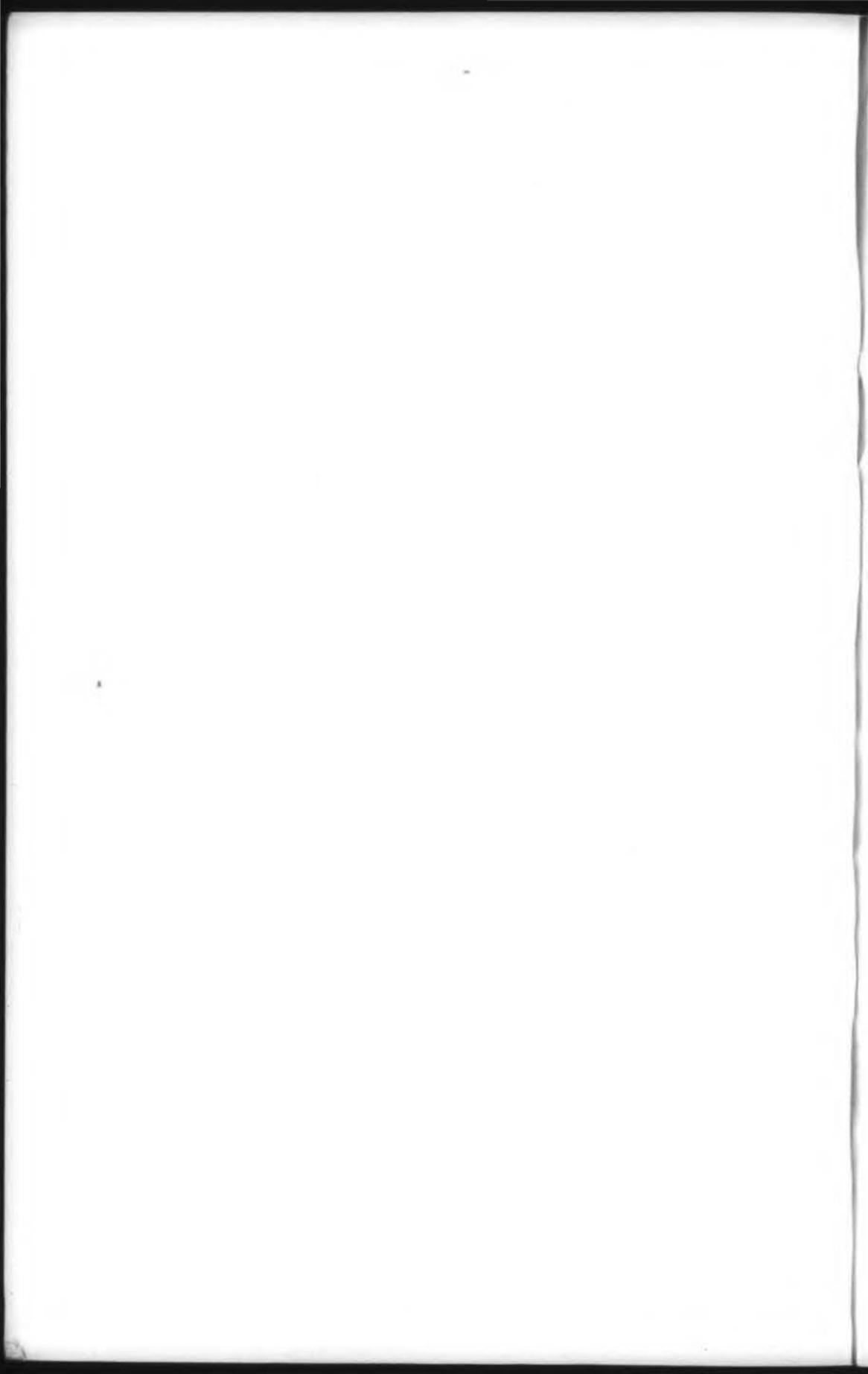
Project 669-V. Road 27 (Tamiami Trail). Dredge in Operation.







Leveling Spoil Bank after Dredging Rock from Canal.





Shaping Road-bed in Tamiami Trail. Note all Material is Rock.

